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**CCL 27/09/2022 - PUBLIC EXHIBITION OF THE DRAFT HARBOUR  
FORESHORE MASTERPLAN**

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**ITEM-71**      **Attachment A:**      CCL 27/09/22 - Draft Harbour Foreshore  
Masterplan

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draft

Newcastle HFMP

# Harbour Foreshore Masterplan



## Acknowledgment

The City of Newcastle acknowledges the traditional country of the Awabakal and Worimi peoples. We recognize and respect their cultural heritage, beliefs and continuing relationship with the land, and that they are the proud survivors of more than two hundred years of dispossession.

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**City of  
 Newcastle**

TCL acknowledges the collaborative process undertaken with The City of Newcastle, site stakeholders and community.

We thank the involvement with Councillors:

Lord mayor: Councillor Nuatali Nelmes

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Councillor Emma White

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Councillor John Mackenzie

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Councillor John Church

---

Councillor Carol Duncan

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Councillor Declan Clausen

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Councillor Andrea Rufo

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Councillor Peta Winney-Baartz

We would also like to acknowledge the important contribution to the Masterplan of the:

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Community Reference Group (CRG), Including; Helen Cummings, Laura Shane, Garry Fielding, Tracey Stephens, Christine Everingham, Rob Russell, Mark Metrikas, Joe Relic, Neil Slater, Doug Lithgow, Reena Bilen, Justin Nyker, Bruce Cooper, Mik Illett and Jamie Graham.

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Harbour Foreshore Masterplan Inclusive Playspace Design Reference Group (PDRG), including; Reena Bilen, Erin Ralph, Luke Russell, Shellie Smith, Kirsty Russell, Nathan Burford, Katrina Lim, Emma MacBain, Mary Pettett, Heather Richmond, Vanessa Henry, Chloe - Archer's Mum, Alexander Goodwin

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Project Control Group (PCG): Director Infrastructure and Property, Director City Wide Services, Manager Major Events & Corporate Affairs, Manager Assets and Projects, Manager Parks and Recreation

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The City of Newcastle project team Including: Tim Daley, Rachel Mc Conkey, Lynn Duffy, Meaghan Bennett, Sandra McKenny & Natalie D'Arcy.

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Variety - the Children's Charity, Councils partner on planning, consulting and designing the proposed all inclusive playspace. Represented in the project design team by Kim Becherand, Head of Inclusive Play & Lisa Whinnen, Inclusive Play Manager - Projects and their team

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*Bosque Landscape Architecture* (Chris Johnstone)  
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 Variety - The Children's Charity

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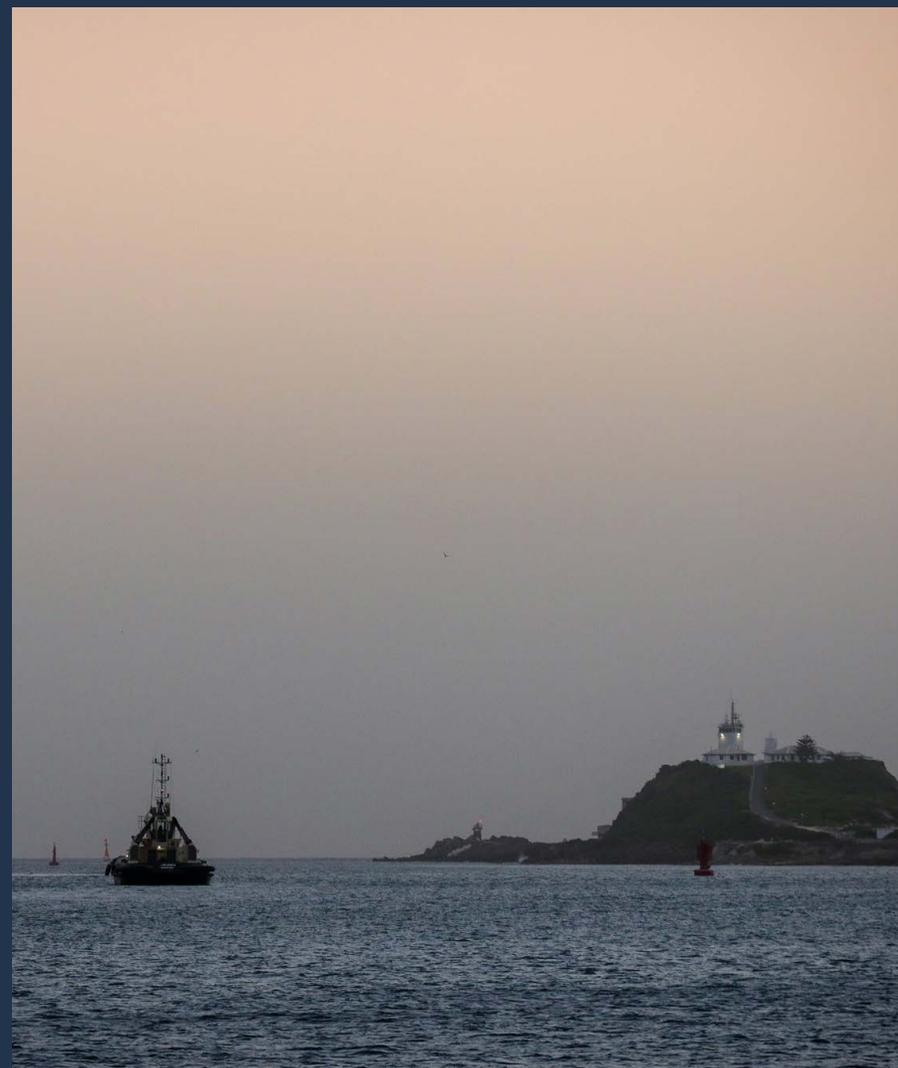
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TITLE	Date	Checked	Approved
Master Plan Report DRAFT 01	10-09-2021	AO	PL
Master Plan Report DRAFT 02	07-09-2022	AO	P L

# 01 Executive Summary

Newcastle Harbour Foreshore is a high quality eleven hectare regional level park adjacent to the city centre of Newcastle. It borders the Newcastle Harbour or Yohaaba, beaches and culturally significant sites of Nobby's or Whibayganba , Fort Scratchley or Tahlbihn, the Convict Lumberyard and Customs House. The park is situated largely on reclaimed land and has a rich cultural heritage from Aboriginal and European settlement. It has evolved through a number of eras including a home for the Awakakal and Worimi peoples for 60 thousand years, a site for coal mining, shipping, managing harbour activities, a power station, bond storage and rail marshalling yards evolving into the open green space we see today.

(Foreshore Plan of Management 2015)



## Introduction

The Foreshore is comprised of the three distinct sectors; Joy Cummings Promenade, Foreshore Park and Camp Shortland. This masterplan applies to the Joy Cummings Promenade and Foreshores Park, It realises a collective community ambition to have an activated, attractive and connected public space along the Harbour edge and Foreshore Park.

The masterplan acknowledges the layers of history that has formed this precinct and has contributed to its popularity with locals and visitors. Equally the plan recognises the great potential for the precinct to improve on these foundations and become a fully linked, attractive and activated sequence of public spaces on a unique active harbour.

The foreshore provides an important connection for the city to its harbour and is host to a range of community and high-profile events. However, roads and infrastructure often dominate the visitor experience and landscape elements such as furnishings, pavements, lighting and facilities are aging and are not commensurate with its civic role. This plan therefore aims to build upon the inherent strengths and attributes, rejuvenate amenities, planting, furnishings and experiences whilst enriching the destination qualities of the setting to entice visitors every day and all year round.

The foreshore masterplan aims to unite its disparate parts, links to its adjacent context, facilitate a diverse range and scale of events and bring new playful experiences, journeys and cultural connections to the community. The plan is informed by patterns of its history, stories of its people and contemporary aspirations.

The masterplan also builds upon the analysis, consultation , objectives and actions outlined in the Foreshore Plan of Management 2015.

The masterplan has also been informed by a detailed staged engagement process that has sought community and stakeholder input and which has guided the collective vision, concept designs and long-term strategy for future projects and development of the precinct.

The masterplan provides an overall vision for the Harbour foreshore that includes key moves and design strategies for the precinct to inform the future development and enhancement over the next 10-20 years.

Key design considerations informing the plan include:

Legibility of access and circulation including universal access measures

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Provision of safe cycling routes along the foreshore

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Indigenous representation

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Connectivity with its urban context and broader public realm

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Recognition of site heritage.

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Tree canopy and strategies to mitigate urban heat island and climate change

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Expand upon the existing planting palette to improve biodiversity and botanical diversity

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Enhancement of outdoor spaces and its programming

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Provision of an all inclusive play space and amenities

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Event use at a range of scales

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Furnishings, pavements and structures

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Signage and wayfinding

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Public art

---

Key views connecting harbour and foreshore

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The masterplan ensures that the public foreshore is reinforced as a centrepiece and significant draw card for Newcastle and is enriched with a range of public spaces and experiences that build upon its cultural stories, inherent strengths and opportunities



Aerial image with project extents highlighted  
(Nearmap, 2020)





Proposed Masterplan with project extents highlighted  
(TCL, 2021)



# 01 Executive Summary

## Building Upon its Great Bones

This masterplan realises a collective community ambition to have an activated, attractive and connected public space along Newcastle's celebrated harbour edge.

The masterplan recognises the foundation strengths and experiences of the foreshore and protects and enhances these qualities.

Key foreshore features such as its open views to water, the continuous greenspace linked by promenades, the generous foreshore park, its heritage elements, open lawns and its variety of social and activity nodes are all recognised as important elements for retention and enhancement.

The masterplan builds upon these qualities with more generous promenades and cycling, with additional links to water, enhanced tree plantings and canopy, improved furnishings as well as proposed new activity hubs, amenities, play spaces and cultural interpretation opportunities

## A Cherished Meeting Place

Newcastle's Foreshore is a cherished meeting place for locals and visitors in the heart of the City. Although a popular strolling, cycling, meeting and event setting, the Foreshore has not provided the furnishings, landscape, lighting and destination qualities commensurate with its important context and role and to entice visitors every day and all year round.

To foster an increased and improved user experience for visitors of all ages, new promenades will connect more people to a range of spaces to meet, socialise and rejoice in the beauty of the harbour setting and Foreshore Park. These broad pathways will also link to shady seating spaces, event lawns, cultural walks, play, dining and various social spaces.

Importantly existing key nodes such as Lynch's Hub and Scratchleys will be enhanced as key destinations, with new nodes at the Merchant Mariners Memorial and Tug Berth Lane offering additional seating and viewing opportunities onto the harbour. In Foreshore Park, the central Railway Carriage Shed precinct will become the social heart with new paths, community lawns, plantings and picnic opportunities.

These will support the new all inclusive play space and water play facility that will become an iconic new destination for locals and regional visitors.



## 01 Executive Summary

### Greening of Wharf Road and Foreshore Park with Sustainability Initiatives

The Harbour Foreshore is valued for its open lawns and tree canopy. These provide a major asset for the community providing a setting with great prospects on to the harbour, a place of refuge and relaxation in contrast to the urban streets nearby.

By contrast, Wharf Road, Tug Berth Lane and parts of Foreshore Park are experienced as open and exposed and diminish the sense of a continuous and inviting green harbour park.

The masterplan recognises the community's desire to maintain these treed lawn spaces and complement them with more shade and seating opportunities to foster greater year-round use and enjoyment. Additional trees and shelter has been suggested in locations that are not impacted by event requirements.

The design proposes transforming Wharf Road into a tree lined esplanade to enhance the overall foreshore experience and provide a suitable entry experience. Additional trees, gardens, and indigenous plantings are proposed for foreshore park and supported by localised water sensitive urban design initiatives.

### Place for Celebration

The masterplan fosters Foreshore Park as Newcastle's premier setting for community use and celebrations. The improved park spaces, tree canopy, furnishings and pathways will further encourage the use of the setting for a range of functions, from small community scale to large regional events.

Within Foreshore Park, a range of spaces are being developed and improved to encourage a range of uses for the community. The proposed regional inclusive playspace will provide a new drawcard and active destination. As will the adjacent proposed community commons centred on the Railway Carriage Shed that will foster family community use with picnics, and bbqs.

Importantly the large event uses held at the park particularly on Shortland Lawn, will be enhanced through improved facilities, pathways and landscape.

The existing Shortland Lawn amphitheatre will be developed to encourage additional event uses as well as day to day uses such as roller and scooter use.

Bridging between Community Commons, the Playspace and Shortland Lawn are two large groves of trees that provide pockets of large canopy trees, providing shade and seating for park users and contributing to the park's overall biodiversity.



## 01 Executive Summary

### Connections

Newcastle's harbour foreshore is a popular setting for strolling and cycling, and forms part of an overall connection from Honeysuckle promenade to the west and linking to Bathers Way, Nobby's Beach and Fort Scratchley. Its popularity however is compromised by pavements that are often too narrow, in particular along Wharf Road creating conflicts between pedestrians, runners, and cyclists.

The masterplan has located a new shared pedestrian / cycling path that links along Wharf Road to Tug Berth Lane and connects to Bathers Way. This path located on the northern edge of the transformed Wharf Road will complement the existing / upgraded harbour edge promenade and cater for safe off-road cycling and pedestrian use. These new facilities will link to improved connections through to the City and into Foreshore Park where a range of new pathways and cultural walks are proposed.

### Cultural Stories

Newcastle's Harbour Foreshore has undergone a remarkable transformation over 200 years.

Prior to European settlement the site was a valued coastal landscape and home to the Awabakal and Worimi peoples.

Its existing use as a valued green park space contrasts with the range of railway, maritime and industrial uses seen on the site since the original settlement of Newcastle. The existing Railway Carriage Shed in Foreshore Park is a potent remainder of uses that once occupied this setting.

These past uses and cultural stories are intended to be captured and interpreted in the proposed cultural walk with a detailed wayfinding and interpretation process to be undertaken following the masterplan. The Cultural Walk, located on the southern edge of Foreshore Park along the original shoreline, is a generous pathway that conveys through interpretation and experience, stories of first nations connection to country within a landscape setting that locates meeting places, boardwalks and wetlands in indigenous flora.





# 02 Project Vision and Brief



## Introduction

TCL, landscape architects and urban designers with Bosque landscape architecture were engaged by The City of Newcastle in 2020 to prepare a masterplan for Newcastle's Harbour Foreshore.

The objective of this project was to build upon the foreshore plan of management for the precinct, consult with community and stakeholders in order to provide a collective vision and long-term strategy.

This process involved a comprehensive community and engagement process complemented with First Nations engagement, Community Reference Groups participation and stakeholder engagement.

The Masterplan process also included a document review, research and detailed site analysis both remotely and on site. Due to COVID19 restrictions, a desktop mapping analysis was initially conducted followed by a visit to site in May, 2021.

Following initial community input a series of design principles were developed to guide design decisions and support and enhance the valued qualities of the foreshore. These principles informed a set of key moves that provided essential steps to guide the precinct to become a rejuvenated and cherished contemporary civic and community destination.

A draft foreshore concept plan and play space concept was developed with input by Variety - the Children's Charity and the Harbour Foreshore Masterplan Inclusive Playspace Design Reference Group. This was exhibited for community consultation and community reference group input in February 2021.

This draft masterplan has been refined following this feedback and represents a collective ambition to build upon the sites great bones and provide a welcoming activated park space with high quality promenades, cycleways, facilities and amenities, active nodes, playspaces and landscapes.

## Program

The program for this Masterplan began in March 2020.



Key: ● Stakeholder group or community engagement  
○ Design Development step

## 01 Project Vision and Brief

### Overview

The Masterplan covers a 13.2 ha area situated in the city centre of Newcastle. It comprises two precincts.

Joy Cummings Promenade/Wharf Road, extending from Argyle Street in the west to Watt Street at its eastern end and comprising the harbour promenade, adjacent parklands and wharf Road.

Foreshore Park, a high quality 7.6ha regional level park, including Tug Berth Lane edging the Harbour and adjacent to nearby beaches and culturally significant sites of Fort Scratchley, the Convict Lumberyard and Customs House. The park is situated largely on reclaimed land and has a rich cultural heritage from Aboriginal and European settlement.

These two precincts were consolidated into a unified public open space from maritime, railway and industrial uses through a design competition was held in the 1980s to convert the area into an open space parkland. In 1985, construction work began on the redevelopment of the site and in 1988 Queen Elizabeth II officially opened the Foreshore as part of Australia's bicentenary celebrations. While this emergence of public open space has become a highly valued community amenity, the precinct as a whole has been in need of a consolidated long term vision. The precinct has the potential to become a major visitor destination and united and appealing draw card for Newcastle.

The purpose of the Masterplan is therefore to guide development over the next 10 years to achieve a Harbour Foreshore Precinct that is:

valued as a destination by residents, visitors and the wider community of Newcastle;

Relevant to a growing and changing population;

Able to respond to increasing and changing community pressures;

Responsive to planned urban renewal and revitalisation of the Newcastle and East End precinct;

Balances the foreshore's competing uses and interests, and accommodates the social, political, environmental and economic changes likely to occur within the planning horizon;

Responsive to forecast climate change impacts, including urban heat island effects;

Celebrates the rich cultural history of the precinct; and

Considers CPTED principles throughout the design.



Sunrise Runners  
*(City of Newcastle, 2021)*

## 02 Project Vision and Brief

### Background Reports

The City of Newcastle adopted the Foreshore Plan of Management in October 2015 which outlines the city's long term vision for the precinct. The Plan of Management identified the need to develop a Landscape Masterplan to guide the future development of the precinct.

The Plan of management provided guidance on key management objectives for the space, including maintaining visual amenity and views, providing appropriate facilities and services and flexible spaces to meet community needs, improving connectivity and activation of the space.

The Plan of Management has been a guiding document to inform key decisions and strategies throughout the masterplanning process.

### Guiding Reports

*Note: Unless specified, these reports are available online*

Foreshore Plan of Management 2015

Disability Access and Inclusion Plan, 2016

Newcastle Local Environmental Plan 2012

Heritage Strategy, 2020

Aboriginal Heritage Management Strategy, 2018

Newcastle City Centre Heritage Conservation Area (Local) [*unavailable online*]

Newcastle Destination Management Plan 2021-2025

Newcastle City Council Events Plan 2016

City Centre Technical Manual

Urban Forest Technical Manual

Cycling Plan - On our bikes 2021 - 2030

Parking Plan - On the Street 2021 - 2030

TfNSW Movement and Place Framework



## 02 Project Vision and Brief

### Objectives

The Masterplan aims to realise the full potential of the foreshore as a premier recreation and event space for the City. The Masterplan explores ways to enable opportunities for locals and visitors to have access, utilise and enjoy the experience of the foreshore.

The Masterplan aims to direct a strong, innovative and creative vision which captures and creatively interprets the essence, history and future of the foreshore. This vision has been informed and directed by a high level of community and stakeholder involvement.

Concurrent to the masterplan process has been the planning, consultation and design for a regional scaled inclusive playspace and water park located in foreshore park. This facility will replace the existing play facility and consolidate the park as a major community family destination. This has been developed in partnership with Variety the Children's Charity.

The briefed objectives for the Masterplan also require a spatially articulate vision for the foreshore including:

A revitalised and aesthetically pleasing precinct in response to the inherent opportunities and constraints;

The balance of facilities, structured and casual spaces and how this supports the visitor experiences available at the park and foreshore;

A unified landscape composed of accessible and diverse spaces;

Legibility and hierarchy in the proposed elements defining the landscape;

Pedestrian and cycle connectivity to surrounding facilities and communities;

Opportunities to explore the diversity of visitation, both casual and event-based;

Expands upon the quantity and diversity of park spaces available for the community

Balancing recreation and the staging of major events;

Protection and enhancement of amenity, environmental, historical and conservation values;

Sustainable water management;

Adaptive and responsive to a shifting climate;

Celebrating the distinctive character and heritage of the Harbour Foreshore Precinct; and

Integration with design outcomes of the wider public domain of the East End;

A regional-scale water and play space commensurate with the rich and vibrant histories of the Foreshore



## 02 Project Vision and Brief

### Key Challenges

The Key challenges identified in the brief for the Masterplan include:

Aging infrastructure throughout the precinct.

---

Cyclist and pedestrian connectivity along the foreshore with meaningful connections to other precincts throughout the city. Current major conflicts occur between Honeysuckle promenade and Watt Street.

---

Historic elements throughout the landscape inclusive of various memorials along the foreshore requiring retention or interpretation.

---

Need for infrastructure to support major regional events, without jeopardising neighbouring activities within the foreshore and park.

---

Need for recreational infrastructure to support increasing population, shift to vertical living and changing demographics.

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Competing use demands.

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The harbour foreshore is an important recreational asset for Newcastle and will have increasing pressure from the community as a passive and active recreation space. The masterplan should explore and critique the range of recreational opportunities provided and examine whether the current balance of structured and unstructured space is optimal.

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Investigation of the local Indigenous culture is required to ascertain the potential for interpretation of the park's cultural values with the possibility of incorporating indigenous themes throughout the park.

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All issues will be considered with the amenity of current and future residents in mind. Noise, light, traffic, views and any other impacts should be considered, as well as the additional increases to amenity arising from park improvements.

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Achieve increased canopy cover where possible

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### Opportunities

The Key opportunities identified in the brief for the Masterplan include:

Creating an inner city parkland to meet the recreational and social needs of the community that differentiates itself from neighbouring facilities.

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Provide improved linkages to Bathers Way and Honeysuckle precincts and the CBD

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Celebrate the foreshore as a major recreational precinct.

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Major events space close to the city and public transport.

---

Integrate community facilities including all abilities play space and water play.

---

Historical influences and cultural elements celebrated within the foreshore.

---

Explore opportunities to provide an integrated community building used to service the park and hold minor functions.

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Explore opportunities to increase night-time activation of the foreshore.

---

Explore opportunities to conserve and protect significant trees which contribute to the foreshore's significant visual amenity.

---

Generating revenue streams.

---

Diversifying the foreshore spaces and creating greater texture and interest in the landscape, within a context of unifying the Park precincts.

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Improved environmental outcomes, increased tree and ground cover planting and best practise WSUD, building ecological biodiversity, carbon capture and mitigating heat island effects.

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# 03 Foreshore Narratives



## Culture

The Foreshore is in the traditional country of the Awabakal and Worimi peoples, where to this day, they maintain a strong connection with the land, the river, the coast and the ocean.

Historically, the Awabakal and Worimi peoples were deeply connected to the rhythms of the coast and river which were intrinsic to the cultural life of community and its provision of sustenance, shelter and stories. The site where the river meets the ocean would have been an important place of gathering.

Today, meeting along foreshore remains an important part of the life of the City.

Newcastle's and the foreshore cultural narratives are also evident in the stories of maritime, industrial and railway uses that have occupied the foreshore over many years since European settlement. The diverse migrant communities that settled here to work have been a major contribution to Newcastle's society and economic offerings.

The Foreshore can showcase the rich and diverse, past and present cultural narratives. Collaboration with the Awabakal and Worimi peoples has been an important part of the masterplan and ongoing engagement and partnership with these communities as designs are further developed will be important as with other cultural groups of the City



## 03 Foreshore Narratives



### Heritage

Newcastle's multilayered heritage is evident in the precinct through the architecture and streetscapes and has also informed the arrangement and extent of the various existing park spaces.

Newcastle's colonial heritage dates back to September 1797 and subsequent settlement in the following years.

The harbour foreshore is home to many significant heritage buildings, elements and monuments.

Themes of marine, lumber, power, industry, commerce resonate across the site. These important foundation uses and narratives are important themes unique to this place and can be protected and enhanced in the visitors experience through subsequent detailed design and interpretation.

In 1973 the Newcastle Green Ban Action saved the site and East End from development and allowed the foreshore industrial area to be transformed into public open space.



### Community

The harbour foreshore is a natural magnet for the Newcastle community, located in the heart of the city, on a working harbour and near important heritage sites and beaches. The expansive green park spaces and linking paths have attracted visitors that seek social engagement in a powerful setting or respite from more urban contexts. The harbour foreshore has provided a range of recreational opportunities, whether walking, exercising, cycling, playing, dining or picnicking.

Coming together to enjoy the theatre of community life, in all its forms, in a beautiful setting is an important guiding narrative for the project.



## Green

Newcastle is a working city, with its architecture and landscapes informed by the practices and patterns of the various industrial and maritime uses. More recently, the City has been planned with an awareness of the impacts of climate change, the need to mitigate against urban heat island effects and the benefits to community and environment of sustainability initiatives.

The masterplan can play a vital role in contributing to an enhanced environmental condition for the harbour foreshore. Through improved tree planting, water sensitive urban design, biodiversity initiatives and sustainable material choices, the harbour foreshore can provide improved community experiences, within a beautiful, shaded and diverse landscape.

### 03 Foreshore Narratives

#### Phase 1 Community Engagement - Values, Uses and Ideas

The aim of this phase of engagement was to discover what the community love about the Foreshore and seek ideas for what they'd like to see here in the future.

Phase 1 engagement took place in June and July 2020. Due to COVID-19 restrictions, this primarily involved broad online consultation via a survey and online interactive mapping tool, and engagement with schools in the local government area (LGA), in partnership with Variety –the Children's Charity. Over 1,000 people provided feedback.

The key things people told us they loved about the Harbour Foreshore included:

- Harbour views
- Grassy areas / green spaces and trees & gardens
- Wide paths and wide openspaces
- Access to nearby restaurants and cafes

The main things the community told us they wanted to see more of included:

- Trees and shade
- Accessible toilets
- Pop-up events and activations
- Picnic tables and gardens

People like playspaces with:

- Areas for different age groups to relax and play in
- Climbing agility and challenging play elements
- Sensory spaces, nature play and quiet spaces are preferred by people with a disability and/or their carers, family and friends.

In addition to the broad engagement activities, a community reference group (CRG) was formed with key stakeholder groups and community members. The CRG met in August 2020 to discuss visions for the project. Informed by community feedback, a preliminary concept for the Foreshore Park precinct of the Harbour Foreshore was developed, including a detailed playground concept plan as mandated by the 2015 Foreshore PoM. The CRG met again in December 2020 where an early version of the preliminary concept was presented for feedback. Members of the project team subsequently met with individual CRG members to further discuss their feedback. The concept plans were also shared for feedback with Year 6 students at St Therese's Primary School in December 2020.

Online Survey    Online mapping tool    School workshops    1,000+ responses



### 03 Foreshore Narratives

*City of Newcastle and Variety – the Children's Charity committed to involving the community to help guide and shape the development of the Harbour Foreshore masterplan. To achieve this, a two-phase engagement program was designed to maximise opportunities for community involvement.*

**City of Newcastle**



## 03 Foreshore Narratives

### Phase 2 Community Engagement - Feedback on preliminary concept for Foreshore Park

The aim of this phase of engagement to gain broad community feedback on the preliminary concept design for Foreshore Park and experiences of walking and cycling along the Harbour Foreshore. The feedback gained was used to help refine the draft masterplan and inclusive play space concept design.

Phase 2 community engagement activities were carried out from 16 February to 5 March 2021. There were a range of ways for people to have their say on the preliminary concepts for the Harbour Foreshore masterplan:



**Drop-in information sessions**  
250+ attendees across three sessions

Feedback at two of the sessions was positive with a broad cross-section of the community saying the project will be great for our city.

Local residents raised concerns around:

- location of the playspace
- usage of the Rail Shed & amenities
- existing community garden
- location of the proposed cafe impeding sight lines
- traffic and parking, including current 'hoon' activity, and
- inadequate consultation.



**Survey**  
194 surveys completed about walking and cycling on Wharf Road



Opinion divided on whether the 30km/h on Wharf Road should be retained



Most would like to see an additional off-road cycle friendly path along Wharf Road.



**Have Your Say webpage**  
2,911 visits  
244 online polls completed

**77%**

said the preliminary concepts looked great or OK



**Social media**  
Post on CN pages & large media outlets

**1000+ 'likes' & 'loves'**

across two CN posts and two posts from large local media outlets



**Social Pinpoint**  
256 comments or ideas received on interactive maps

**People would like to see more:**

- Trees and shade
- Facilities for skating and scooting

**Improvements to consider:**

- Switch location of playspace with Picnic/BBQ Lawn
- Move café to preserve view lines from stairs to the harbour
- Repurpose frog pond for skating and scooting
- Retain existing community garden



**Submissions**  
31 written submissions

**Key issues raised in submissions:**

- playspace too close to residents and businesses, with associated noise impacts and loss of passive recreation
- inadequate parking and increased traffic
- amount of event space
- calls to retain the existing community garden
- preference to retain frog pond as a water feature

## 03 Foreshore Narratives

Using community feedback, a number of changes were made to the preliminary concept for Foreshore Park including:

Relocating the playspace next to the Wharf Road carpark, away from nearby residents and businesses and closer to accessible parking spaces.

Centralised amenities with optional café/kiosk

Dedicated area for roller skating and scooting

Improved connections to Tug Berth Lane.

The revised concept for Foreshore Park, and the Wharf Road precinct concept, were presented to the CRG for feedback in July 2021. The concepts were also shared for feedback with Year 6 students at St Therese's Primary School in August. Overall, the concepts were positively received, and feedback received from these groups was used to help further refine the draft Harbour Foreshore Masterplan.

Thank you to everyone who had their say during development of the Harbour Foreshore masterplan.

For more information on Harbour Foreshore community engagement activities, please visit [newcastle.nsw.gov.au/yoursayopportunities](https://newcastle.nsw.gov.au/yoursayopportunities) for community involvement.



# 04 Context and Site Attributes



## Site Analysis

Initial site analysis of the precinct involved remote aerial mapping as well as site visits to the area.

Mapping included pedestrian and vehicular access and circulation; heritage overlays and historical chronology; current building occupancy and ownership; figure ground; events mapping; landscape materiality, lighting and furnishings; built edge conditions and existing softscape.

In addition to aerial mapping, a 'walking the site' analysis was also conducted, which documented a pedestrian's first impressions perspective of the experiential qualities of the site. This is an essential component of understanding what makes a public space successful to users.

Refer appendix for further site analysis and the heritage report.



Site and surrounds  
(Nearmap, 2020)

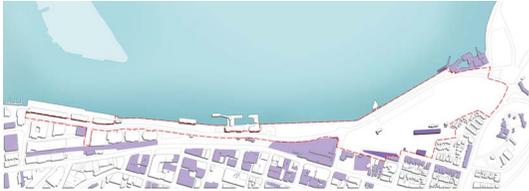
# 04 Context and Site Attributes



(Site Context) Landscape and Ecology



Topography



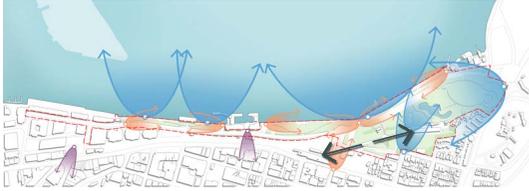
Heritage



Cycle (Proposed) Network



Public Transport Network



Views



Events

## 04 Context and Site Attributes

### City Context

The Harbour Foreshore comprising Joy Cummings Promenade/Wharf Road and Foreshore Park are high profile harbourside parks located adjacent to Newcastle's CBD and the iconic East End. It is a location rich with both indigenous and European history as well as the natural beauty of the harbour and Newcastle coastline. The site is highly accessible with light rail, ferry and bus services within close walking distance. The site is at the apex of two significant walkways and highly used shared pathway networks: The Bathers Way that wraps along the City's coastline from the South and the R6 harbourside pathway that connects the foreshore to the western suburbs.

The harbour edge once cut off from the city by the railway is now linked through connections from east to west at: Newcomen St, Market St, Perkins St and the new connection between Darby St and Argyle St at Darby Plaza.

With vehicle connections accessing the site along Wharf Road from the east at Merewether Street and west at Nobby's Road and Shortland Esplanade as well as links via Watt Street and Argyle, Street.



## 04 Context and Site Attributes

### Site Context

The site located on largely reclaimed land on Newcastle's harbour, extends 1.7 kilometres in length, and comprises an area of 13.2ha.

Wharf Road is comprised of the Joy Cummings promenade, adjacent narrow parkland of Foreshore Reserve, with the two lane wharf road on the southern boundary. Key nodes include the Queens Wharf Precinct, Scratchleys Restaurant and Lynch's Hub.

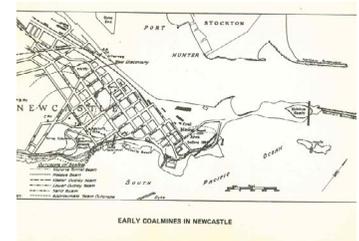
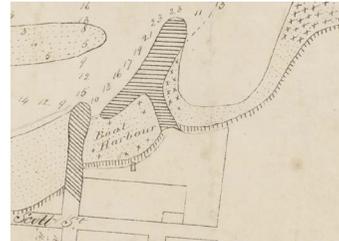
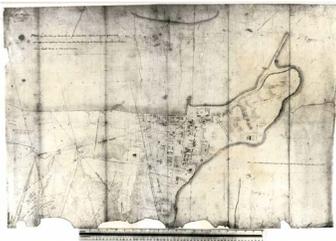
On its southern edge is the newly completed Market Street Lawn and reinterpreted Newcastle Railway Station public open spaces.

Wharf Road Precinct connects to the Honeysuckle harbourside developments on its west, whilst Foreshore park and Tub Berth Lane are located at the eastern end of the precinct.

Foreshore Park a 7.6ha regional scaled park on the site of the former Newcastle railyards. It is situated adjacent to key Newcastle features such as the Macquarie Pier & Nobbys headland, Camp Shortland, Fort Scratchley and Nobbys beach.

On the southern edge of the park is the Newcastle East residential area as well as are significant heritage elements such as the Customs House Plaza, Paymasters, Former Bond Stores and the Convict Lumberyard.

Foreshore Park features large open lawn areas with treed areas and a path network. Key features include the open Shortland lawn and amphitheatre, the frog pond, historic Railway Carriage Shed, Sandhills community garden, local play space. Tug Berth Lane is located to the north of the Park adjacent to Wharf Road and the harbour.



## 04 Context and Site Attributes

### Heritage Context

The study site straddles several heritage conservation areas defined by the Newcastle LEP and the NSW Heritage Act. The western arm of the site falls within the Newcastle CBD Heritage Conservation Area, while the bulk of the current Foreshore Park area of the site falls within the Newcastle East Heritage Conservation Area, of which there are three precincts. The very east of the study site extends into the State heritage listed Coal River Precinct.

The European heritage layer of the study site is varied and rich, with extant built fabric telling the story of Newcastle's history as a convict and coal mining harbour city. A number of significant structures are located within the study site however, a large amount of the physical heritage contributing to the site is the fabric to the south - the backdrop of the city.



 Newcastle CBD Heritage Conservation Area

 Coal River Precinct

 Newcastle East Heritage Conservation Area

 Heritage sites within the project scope

Above: Heritage Context and Precincts  
(Heritas, 2020)

Within the site are several heritage listed items related to the historic harbour interface, including the Earp Gillam Bond Store Precinct and Bond Street Steps (c1890) and the former John Bull Warehouse (1895). The Railway Carriage Shed, and two small, now altered amenities buildings (1943 and 1945) also sit within the study site. These structures all relate to the interaction between the settlement of Newcastle and the harbour, specifically the transport and storage of goods.

The southern boundary of the study site with the city contains an abundance of heritage fabric also relating to the harbour interface, including Customs House (1877), Newcastle Railway Station (from 1858) and Signal Box (1936), Paymasters Cottage (1879), the Earp Gillam (1888) and Stanton Catchlove (1890) bond stores, and the former Coutts Sailors Home (1882). Adjacent to the study site, the Convict Lumberyard (c1818) also presents an urban parkland space with links to the convict history of the harbour foreshore.

Beyond commercial uses, a number of heritage listed residential sites add to the character of the inner city harbour interface including, most prominently, the Stevenson Place Precinct – a two blocks long row of terraces overlooking the current Foreshore Park.

The western arm of the study site is highlighted by Argyle House, the early Victorian cottage originally the offices of the Australian Agricultural Company in Newcastle. Similar to the eastern end of the study site, this western arm is also characterised by numerous heritage items outside of the site boundary but forming a backdrop to the study site, including large commercial buildings such as the former Scott's Department Store building, former Commonwealth Bank building, the dominating former warehouse structures of Beberfauld's and R. Hall & Sons, and the former Newcastle School of Arts building.

The historical interface with the harbour itself is a more diluted remnant however no less significant. Out of the study site boundary but still able to characterise it are the sites of the Pilot Station, Boat Harbour and Boat Sheds (from 1866), Fort Scratchley (1882) and Nobbys Lighthouse (1857). Historical mapping also confirms elements within the study site but no longer visible, such as the original shoreline and the extent of the beach sandhills, over which timber plank pathways once allowed for pedestrian movement between the city and the eastern promontory of the settlement. A c1840 ballast wharf jetty is believed to be remnant in the area of the 1945 amenities building.

- Heritas, 2022

Opposite page: Foreshore Park Heritage Context  
and Buildings (Heritas, 2022)

\*Note: Building demolition works within the East End Heritage Conservation Area require a Heritage Impact Assessment and Development Approval.



- 01: Heritage Item 1491 (Local) Former John Bull Warehouse  
 02: Heritage Item 1492 (State SHR 0762) Former Earp Gillam Bond Store  
 (Earp Gillam Bond Store Precinct)  
 03: Railway Carriage Shed  
 04: Former Railway Yards Amenities (West)  
 05: Former Railway Yards Amenities (East)

- Foreshore Precinct-Newcastle East HCA  
 Historic Core Precinct-Newcastle East HCA  
 Residential Precinct-Newcastle East HCA

## 04 Context and Site Attributes

### Landscape Context

Wharf Road Precinct is characterised by a narrow parkland space located between the road and the harbour promenade. The majority of the park is comprised of open lawns and canopy trees with species including *Auracaria heterophylla* (Norfolk island pines), *Araucaria columnaris* (Cook pines), *Casuarina Glauca* (Swamp sheoak), *Phoenix canariensis* (Canary island date palm), *Phoenix reclinata* (Senegal date palm) and *Ficus rugbignosa* (Port jackson fig). Some garden spaces are located sporadically along the foreshore comprising of species such as *Lomandra longifolia* (Mat rush) and *Crinum pendunculatum* (Swamp lilly).

Foreshore Park is a large 7.6ha landscape, located on reclaimed land on the site of the former Railway marshalling yards comprising, large open lawns and mostly informally arranged canopy trees.

A range of trees are considered significant including an avenue of *Auracaria heterophylla* (Norfolk island pines) located on axis with the Railway Carriage Shed and *Araucaria columnaris* (Cook pines)

Additional species used within the park include:

*Casuarina cunninghamiana*, *Banksia integrifolia*, *Cupaniopsis anacardioides*, *Triadica sebifera*, *Acmena smithii var. minor*, *Pittosporum undulatum*, *Angophora costata*, *Melia azedarach*, *Corymbia maculata*, *Brachychiton acerifolius*,

*Tristaniopsis laurina*, *Angophora costata*, *Acacia sophorae*, *Phoenix canariensis*, *Callistemon citrinus*, *Casuarina glauca*, *Ficus rubiginosa*, *Phoenix reclinata*, *Syzygium smithii*, *Syzygium leuhmannii*, *Waterhousea floribunda*, *Platanus orientalis* "Digitata", *Viburnum tinus*, *Melaleuca quinquenervia*, *Agathis robusta*, *Syagrus romanzoffiana*, *Cupaniopsis anacardioides*, *Murraya paniculata*, *Ficus carica*, *Inga vera*, *Hibiscus tiliaceus*, *Triadica sebifera*, *Olea europaea subsp. Cuspidate*, *Metrosideros excelsa*, *Acacia saligna*, *Banksia serrata*, *Howea forsteriana*, *Erythrina crista-galli*, *Casuarina equisetifolia*, *Callistemon 'Kings Park Special'*, *Ligustrum lucidum*, *Lagunaria patersonia*, *Washingtonia filifera*.

At the centre of the park is the Frog Pond, that has been emptied of water and is now a sunken and open concrete basin.

A valued community garden space (Sandhills Community Garden) is located near to the Railway Carriage Shed adjacent to the park works depot.

The park is defined by an escarpment on its southern edge which frames the park spaces and illustrates the original shoreline. Along this edge are undulating lawns and more detailed gardens and plantings.



(City of Newcastle, 2021)

## 04 Context and Site Attributes

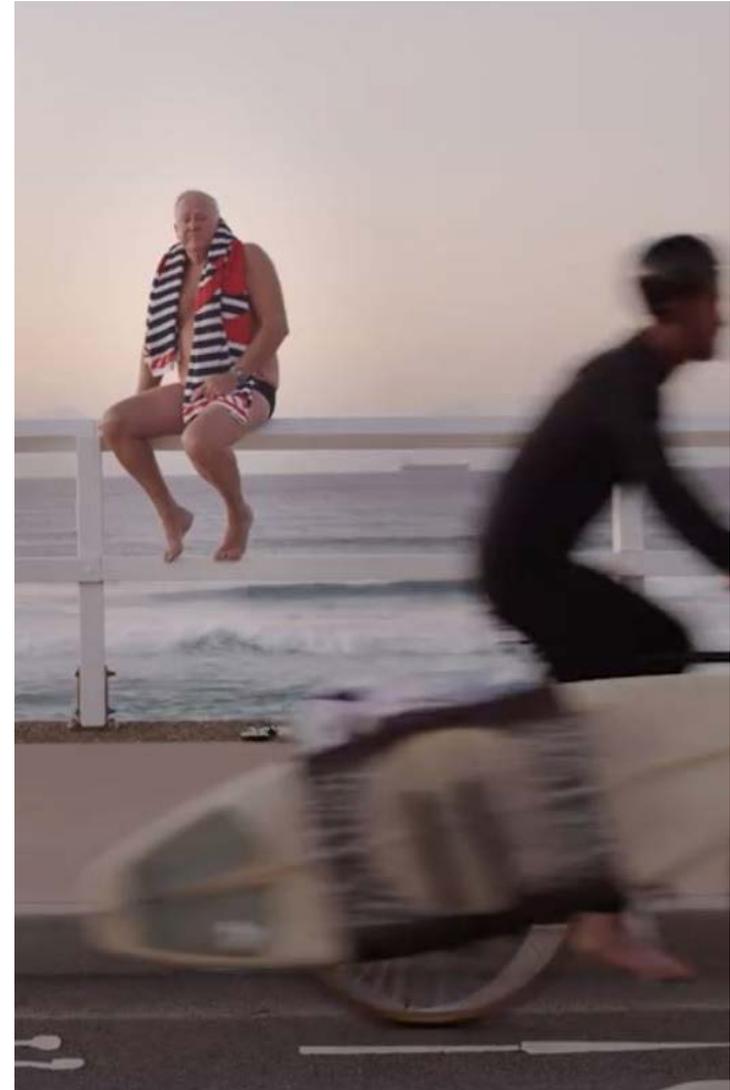
### Connectivity

The harbour foreshore is well connected to its adjacent city context and well served with public transport including ferry, light rail and bus services.

Pedestrian and cycle connections to the site are available along the narrow harbour promenade which links to the western suburbs and east to the Nobbys Beach and beyond.

Pedestrian connection to the site from the city are available at, Nobbys Beach & SLSC, Bathers Way , Fort Scratchley via Nobbys Road, Telford Street , Bond Street, Watt Street, Market Street, Perkins Street, Darby Street, Merewether Street and Honeysuckle Promenade.

Carparking and access is available via parallel parking bays along Wharf Road as well as defined parking areas at Queens Wharf. At Foreshore Park a large carpark exists along Wharf Road, that is also used for events. A large car parking station is located at the corner of Wharf Street and Argyle Street.



## 04 Context and Site Attributes

### Uses

The Harbour Foreshore is an important informal recreation and cultural space for the inner city and the wider Newcastle community. The site is a premier event space for the City, hosting a range of community based and commercial events including Newcastle 500 , concerts, markets and family gatherings.

The area is enjoyed on a day to basis for a range of recreational uses such as strolling, meeting, play, informal sports, picnicking, bbqs, fishing and cycling. Use of the adjacent lawns for informal dining and takeaway meals is a popular activity near to Scratchleys and Lynch's hub.



# 05 Key Moves and Principles



## Principles

The Masterplan identified a series of design principles, to guide the key directions and decisions and to test and validate ideas as they emerge. These principles were developed in response to community consultation and Community Reference Group input.

*1. Reinforce Sense of Place:* Ensuring the masterplanning is of this place, is site responsive and uniquely of Newcastle. With outcomes that celebrate its valued and distinctive qualities.

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*2. Create a Sequence of Destinations:* Paths and promenades should link to a series of places and create interesting and memorable interludes on the journey.

---

*3. Community Facilities and Amenities:* Provision of high-quality public park facilities to serve the community.

---

*4. Flexible Park Spaces:* Ensuring the foreshore is robust and able to perform a multiple of roles and have a variety of functions to best serve many visitors needs over many years.

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*5. Inclusive Access:* Ensuring our foreshore has safe equitable access for all.

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*6. Connected:* Provision of generous safe, walking and cycle networks. That are beautiful and part of a great foreshore experience.

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*7. Management:* Recognizing the success of the harbour foreshore will be, in part, how they are cared for and are organized for events.

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*8. Views and Landmarks:* Ensuring key vistas and panoramas are acknowledged, protected and reinforced. Those chosen should enrich the experience, aid orientation and reinforce a sense of place.

---

*9. Active Nodes:* keeping the foreshore activated and well used through well designed community focused activity spaces.

---

*10. Environment:* To plan for a climate resilient parkland with an enveloping canopy of suitable shade trees that also integrates water sensitive urban design into its landscapes.

---

*11. Heritage:* ensuring we identify values and interpret our pre-colonial and post-colonial stories of place.

---

## 05 Key Moves and Principles

### Key Moves

The Newcastle Harbour Foreshore Masterplan is designed via a sequence of key moves which will define the precinct as a world class waterfront parkland by building upon its valued characteristics, establish key connections, embellish with beautiful trees and plantings and foster a range of community focussed activity and social park spaces.

#### Unite The Foreshore

*Allow Foreshore Park to realise its full potential as the premier connective open space destination for the city of Newcastle. Connect current separate open space assets via new green walks, parks and cycleways.*



#### Establish a World Class Promenade

*A promenade that is generous, active, beautifully furnished and exquisitely lit that links destinations and celebrates the harbour setting.*



### Create a Memorable Cycleway

*Linking a majestic harbour to a dynamic and scenic ocean experience via a seamless new cycling experience.*



### Wharf Road as Great Esplanade

*Transform Wharf Road from an access drive into a memorable tree lined harbour esplanade.*



### Foreshore Park with Memorable Spaces

*Reinforce the foundation patterns of the park we have established with a sequence of defined spaces each with their own role and character.*



## 05 Key Moves and Principles

### Create a Community Heart

*Deliver a memorable themed play and water experience adjacent to a revitalised Railway Carriage Shed and café to become a major community destination and to animate the park all year round.*



### Establish a New Cultural Walk

*Link the park, marking the original shoreline with a new interpretive journey which will celebrate and tell stories of the Awabakal and Worimi and their connection to this place and ensuring we incorporate ecological and water sensitive city initiatives.*



### Enrich the Foreshore with a Botanical Overlay

*Introduce a beautiful botanical and ecological story to the setting, with strong biodiversity values, with shaded spaces and furnishings to encourage year round use*



### Commemorate the Creation of a City

*Celebrate the architectural quality of Customs House and its role in the formation of the city by embellishing the plaza with stores that have driven the formation of the City of Newcastle.*



# 06 Masterplan



## Overview

This Masterplan realises a collective community ambition to have an activated, attractive and connected public space along the Harbour edge and connecting to Foreshore Park.

The Masterplan acknowledges the layers of history that has formed this precinct and has contributed to its popularity with locals and visitors. Equally the plan recognises the great potential for the precinct to improve on these foundations and become a fully integrated, attractive and activated sequence of public spaces on a unique active harbour.

## Major Elements

The report describes the components of the Wharf Road Precinct and Foreshore Park in subsequent chapters. Connecting both spaces are the following key elements and linkages.



## 06 Masterplan

### 01 Pedestrian Connections

A key feature of the Masterplan is the rejuvenation of the harbour promenade to be a world class experience that fosters greater connectivity to its Harbour.

The existing waterside pathway along Wharf Road is upgraded with new pavements, lighting and seating opportunities.

The promenade is extended east along Tug Berth Lane as part of a redevelopment of this space as a tree lined waterfront parkland.

Combined the promenade provides visitors with a generous link from Honeysuckle in the west to Bathers Way in the east and linking to adjacent nodes of activity, amenities, seating spaces and Foreshore Park.

### 02 Cycling Connections

A new separated cycleway is proposed along Wharf Road to alleviate pedestrian cycle conflicts along the Joy Cummings foreshore promenade.

This cycleway will be located on the northern edge of the road and provide a circulation edge between the parallel parking, avenue trees and the foreshore parkland lawns.

This pathway will link to the proposed Tug Berth Lane promenade and shared cycleway, creating a link from Argyle Street in the west to Watt Street in the east.



### 03 Green Connections

The foreshore parklands along Wharf Road are proposed to be extended along Tug Berth Lane to the east to provide a continuous green promenade and park experience linking to Foreshore Park.

Tug Berth Lane, currently an asphalt road will be transformed to a tree lined pedestrian and cycle pathway with new shade trees, open lawns and seating spaces.

### 04 City Connections

The success of the foreshore will in part be due to the way it connects to its city context. Proposed improved connections are proposed at Argyle Street linking to Darby Street, Lynch's Hub to Honeysuckle Promenade, Scratchleys to Perkins Street, Queens Wharf to Market Street, Watt Street to the refurbished Station and Foreshore Park to Tug Berth Lane.



# 06 Masterplan

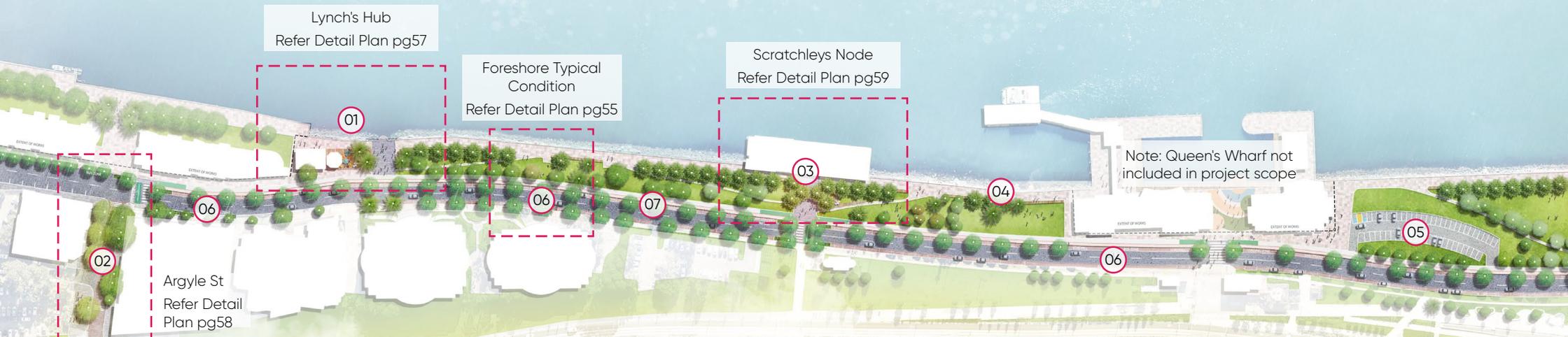
## Wharf Road Precinct

### Wharf Road Precinct

The Wharf Road Precinct is redeveloped to celebrate its location on the Harbour. Upgraded promenade experiences, destinations and safe cycleways provide new links and ways to enjoy the waterside experience. Wharf Road is transformed into an attractive Harbour Esplanade with new avenue trees, pavements and furnishings.

### Carpark Rationalisation

Rationalisation of existing parking bays east and west of Queens Wharf into a consolidated parking area that is more integrated into its landscape context open up the pedestrian space surrounding the building. Rationalisation of the carpark is to be designed and undertaken when existing carpark assets are planned for renewal. Parking numbers to remain the same.



### Legend

- 01 Lynch's Hub
- 02 Argyle Street
- 03 Scratchleys Node
- 04 Joy Cummings Harbour Promenade
- 05 Carpark Rationalisation
- 06 Wharf Road
- 07 Pedestrian/Cyle Path
- 08 Tug Berth Lane

**Pedestrian / Cycle Path**

To alleviate congestion and safety along the harbour promenade a new pathway is located on the northern side of Wharf Road. The path shall cater for bi-directional cycle and pedestrian movements and link from Argyle Street to Tug Berth lane promenade and east to Bathers Way and Shortland Esplanade.



## 06 Masterplan

### Wharf Road Precinct

Wharf Road redeveloped as a memorable harbour esplanade.

Improve the amenity and experience of the environs through new defined parking bays with dedicated WSUD tree planters every second car bay to treat stormwater. This will reduce the scale of the road, create an avenue effect and boulevard driving experience, mitigate urban heat island effect and link to the Foreshore Park landscape.

Pedestrian pavements to be upgraded with new furnishings and lighting.

Provide dedicated widened pedestrian crossings at Argyle, Lynch's Hub and Scratchleys to foster links from the City to the foreshore.

Provide a new pedestrian/cycle way to the north of Wharf Road at the Foreshore Park interface to foster safe cycle links from Honeysuckle Promenade to Bathers Way.

#### Legend

- 01 Integrated Street Tree Planting
- 02 Parallel Parking
- 03 Generous Pedestrian Crossings
- 04 Shared Cycleway
- 05 Activated Edges





# 06 Masterplan

## Foreshore

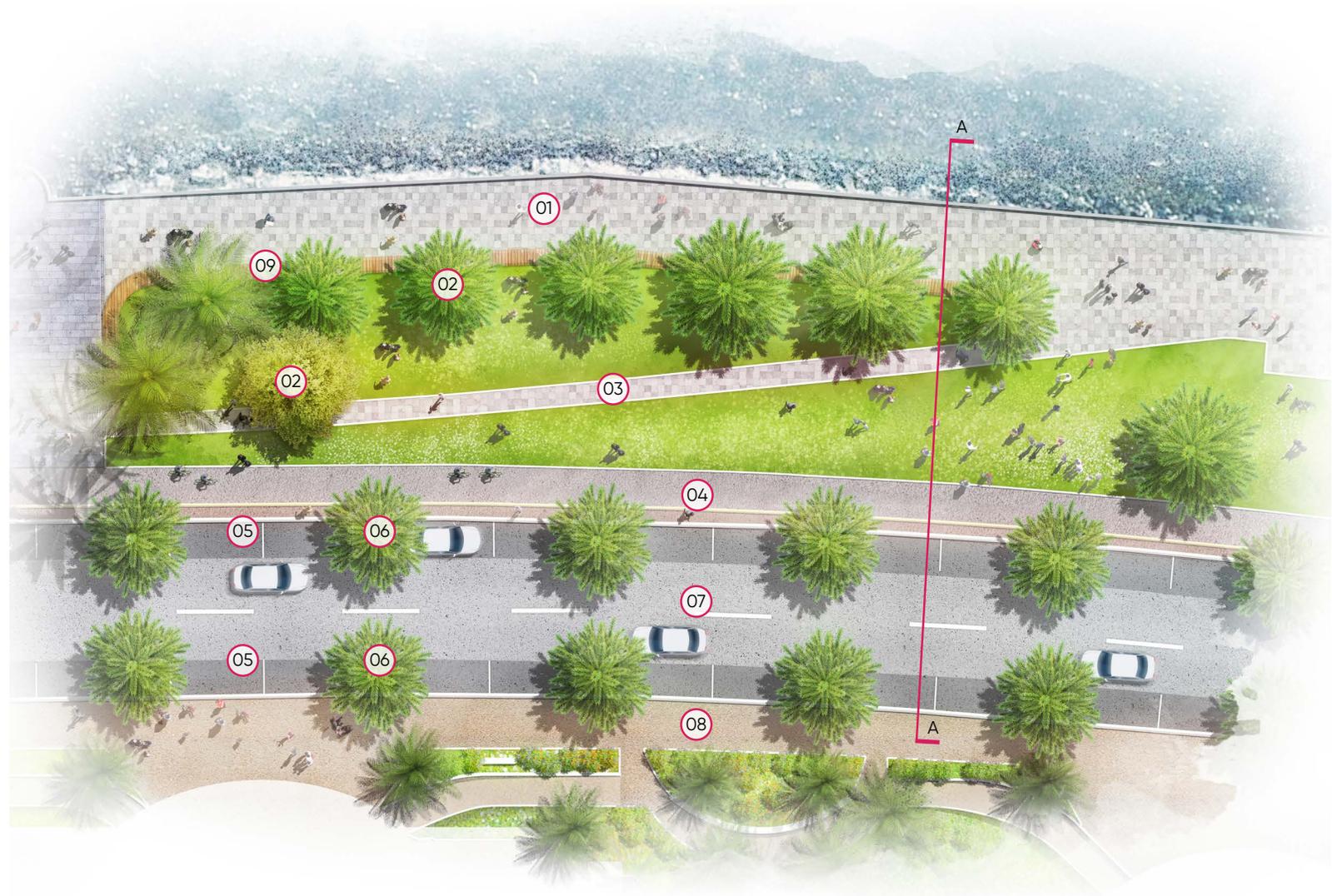
### Typical Condition

Improve promenade experience along Harbour edge through upgraded and widened pavements, adjacent lawn renovation and a new continuous seat edge facing onto the harbour.

Improved safety and amenity lighting to enhance the night time experience.

#### Legend

- 01 Harbour Promenade
- 02 Existing Trees
- 03 Upgraded Existing Pathway
- 04 Shared Cycleway with adjacent Pedestrian Zone
- 05 Parallel Parking
- 06 Street Tree Planting
- 07 Two-lane Bidirectional Road
- 08 Upgraded Existing Footpath
- 09 Continuous Seating Edge



Foreshore Typical Condition (Plan)  
(TCL, 2021)

Scale 1:400 @A3





Foreshore Typical Condition (Section A-A)  
(TCL, 2021)

Scale 1:200 @A3

## 06 Masterplan

### Lynch's Hub

Improved plaza, decked seating area, upgraded lawn spaces, seating and promenade.

Feature lighting to accentuate night-time experience.

Proposed cycle / pathway along Wharf Road and dedicated crossing.

Raised Ship builder's memorial to be flush with new promenade pavements.

#### Legend

- 01 Harbour Promenade
- 02 Raised Memorial
- 03 Shared Cycleway with adjacent Pedestrian Zone
- 04 Parallel Parking
- 05 Street Tree Planting
- 06 Upgraded Existing Footpath
- 07 Decking with AI Fresco Dining
- 08 Lawn with Tree Planting



Lynch's Hub (Plan) Scale 1:400 @A3  
(TCL, 2021)



## 06 Masterplan

### Argyle Street

Foster greater connections between the City and Harbour by widening pedestrian path and planting of new trees with integrated water sensitive urban design (WSUD) on its western edge.

Maintain two laned access to connect to access properties and carpark.

#### Legend

- 01 Widened Footpath
- 02 Proposed Tree Planting and Integrated WSUD
- 03 Generous Pedestrian Crossing
- 04 Stronger Pedestrian Connection to Darby Plaza
- 05 Threshold Paving to Argyle St to reinforce Quiet Way street



# 06 Masterplan

## Scratchleys Node

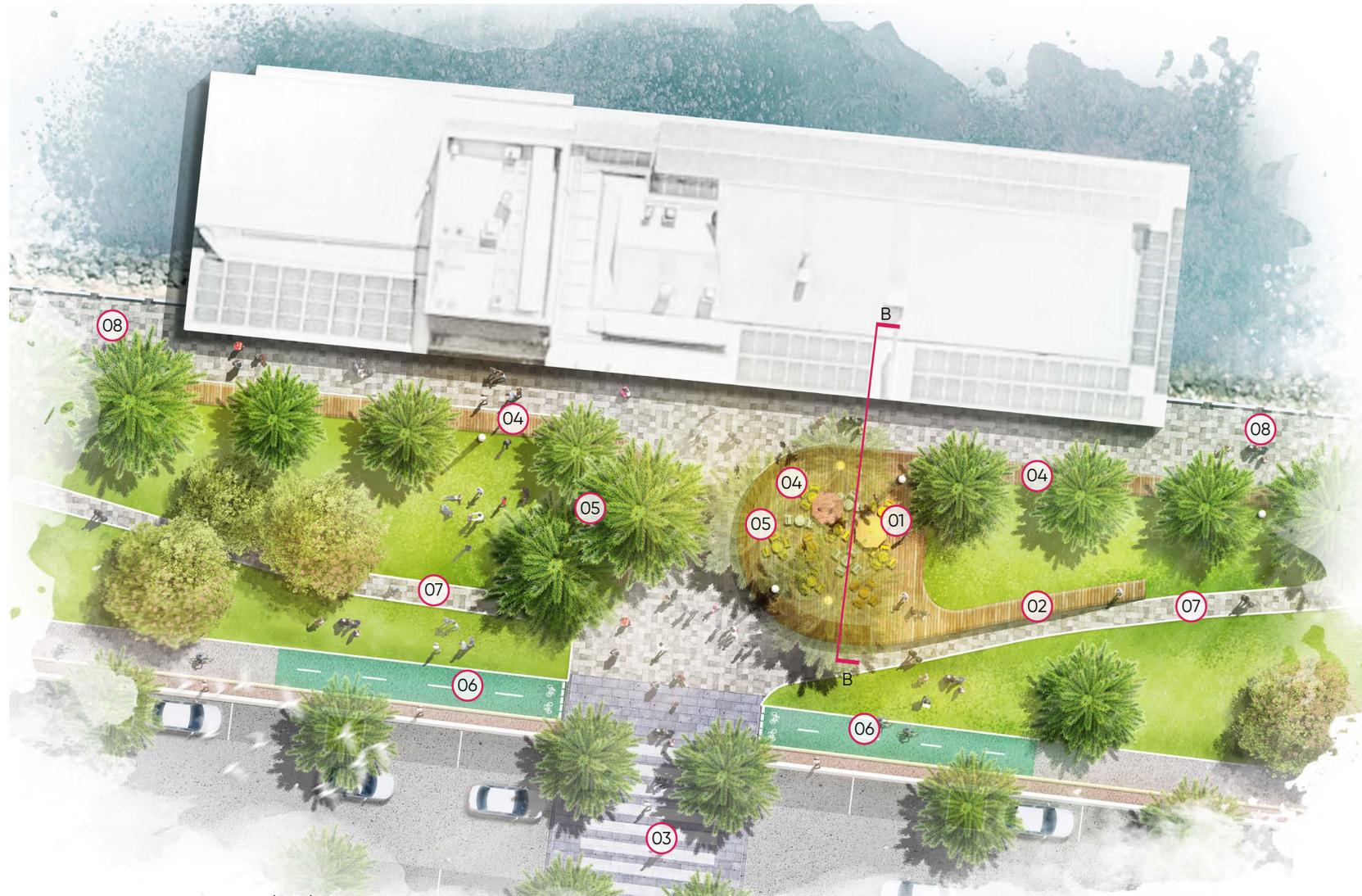
Create an attractive destination by improving the plaza, with decked seating area, upgraded lawn spaces, seating and promenade.

Provision of new feature lighting to accentuate the night-time experience.

Proposed cycle / pathway along Wharf Road with widened dedicated crossing linking to Harrys' Café de Wheels and Scott Street.

### Legend

- 01 Timber Decking with Alfresco Dining
- 02 Accessible Ramp
- 03 Generous Pedestrian Crossing
- 04 Seat Edge
- 05 Existing Pines
- 06 Shared Cycleway
- 07 Upgraded Existing Pedestrian Path
- 08 Harbour Promenade



Scratchleys (Plan) Scale 1:400 @A3 (TCL, 2021)



# 06 Masterplan



Scratchleys Node (Section B-B)  
(TCL, 2021)

Scale 1:100 @A3

## 06 Masterplan

### Scratchleys Node

#### Legend

- 01 Timber Decking with Alfresco Dining
- 02 Accessible Ramp
- 03 Seat Edge
- 04 Existing Pines



## 06 Masterplan

### Tug Berth Lane

Develop Tug Berth Lane as a linear park, promenade and cycleway with new shade trees and lawns right to the water's edge. This shall provide an attractive harbour walk that connects Joy Cummings Promenade to Nobby's Beach, Fort Scratchley and Foreshore Park.

This welcoming harbour destination will be complemented with terraces shelters and seating opportunities to foster enjoyment of the harbour and invite a range of social and recreational activities.

Connect Tug Berth Lane with Foreshore Park via a dedicated pedestrian crossing at the mid point along the promenade.

#### Legend

- 01 Cycleway and Footpath
- 02 Terrace and Shelter
- 03 Lawn edge
- 04 Generous Crossing
- 05 Indicative location for Iconic Newcastle Sign \*Subject to further detail during staged design development





# 06 Masterplan

## Tug Berth Lane Condition A



Tug Berth Lane Condition A (Section C-C)  
(TCL, 2021)

Scale 1:100 @A3



### Tug Berth Lane Condition B



Tug Berth Lane Condition B (Section D-D)  
(TCL, 2021)

Scale 1:100 @A3

## 06 Masterplan

### Foreshore Park Precinct

Foreshore Park is planned to reinforce its strong bones, patterns and use to become a cherished community destination and event setting. Through a new inclusive play space, water play, upgraded pathways, interpretation, furnishings, technology, gardens and trees, visitors will enjoy a range of attractions facilities and destinations.

#### Legend

- 01 Customs Plaza
- 02 Cultural tWalk
- 03 Variety Livvi's Place Playspace
- 04 Amenities Pavilion & Cafe
- 05 Railway Carriage Shed (to be retained)
- 06 Community Commons
- 07 The Groves
- 08 Shortland Lawn Event Space
- 09 Amphitheatre
- 10 Sandhills Community Garden (to be retained)



## Events

The Harbour Foreshore precinct is one of Newcastle's most iconic locations with its close proximity to the harbour, beaches, cafes, restaurants and public transport.

The precinct contains several venues that can be utilised individually or in combination to attract and facilitate everything from small scale community activities to nationally recognised events. These events greatly contribute to the city's visitor economy and its regional and national identity.

The Masterplan recognises the importance of the Harbour Foreshore precinct as a premier tourism and event hub for the city. The Masterplan has reviewed all past and proposed events to produce a plan that accommodates bookable areas and events of all scales within the park. Event requirements such as laydown and access areas and offsets necessitate limited obstruction (i.e. trees) within the event footprint and the new plan will increase the flexibility of the park to deliver these events while celebrating the qualities that make the Harbour Foreshore precinct unique and treasured by the community.



## 06 Masterplan

### Customs House Plaza

The Customs House is an important Newcastle building, designed in 1877 by James Barnet. The building and plaza have a strong relationship and axial view to the harbour.

The area has the potential to form an important orientation and entry experience to Foreshore Park, from Watt Street, nearby public transport links and the city.

Although the axial and visual link to the harbour, framed by existing trees is currently successful, the existing sunken plaza is not used effectively for day to day use or managed for events.

The Masterplan proposes to create a more generous threshold to the park in front of the Customs House building to accentuate arrival, ease orientation, and link to the proposed play space, cultural walk and great park setting. The

existing sunken pavement will be removed and levels brought to be flush with adjacent lawn surfaces with a new central path linking to the harbour and centred on the building.

In association with the great lawn area, new seating and lighting will foster greater use both day and night. Whilst the level surfaces will be able to be used for pop-up events, or more formal community celebrations.

A commissioned sculpture is proposed on the lawns in and adjacent to the existing trees, to highlight the arrival experience and communicate the important link this area has to the harbour and to Newcastle's colonial history. The artwork could potentially be a fun and interactive piece that is illuminated to create a memorable night time experience.

#### Legend

- 01 Axial link to Harbour
- 02 Level Plaza
- 03 Upgraded Existing Water Feature
- 04 Extended Paving
- 05 Sculpture and Artwork
- 06 Seating under existing trees
- 07 Feature Entrance to Cultural Walk
- 08 Interpretation of Former Shoreline
- 09 Widened Pedestrian Crossing
- 10 Future Pedestrian Crossing to be investigated to better link Foreshore Park with Rail Station Redevelopment



🕒 Customs House Plaza (Plan) Scale 1:1000 @A3  
(TCL, 2021)

## 06 Masterplan

### Cultural Walk

Establish a new destination and journey in Foreshore Park that follows the line of the original shoreline and narrates cultural stories of Newcastle and its Awabakal and Worimi peoples.

The walk extends from Customs Plaza to Fort Scratchley, connecting to the railway carriage shed, community commons, Shortland lawn and amphitheatre. The path would comprise boardwalks, lawns, meeting places, artworks, indigenous plantings, gardens and wetlands as part of the journey.

This interpretive and experiential journey will relay Awabakal and Worimi stories of the foreshore and greater context.

Conceptually arranged around a journey from the River to the Coast, themes for interpretation could include, sand dunes, frog pond, bush tucker, naming, indigenous flora and biodiversity, ceremony and story telling.

Detailed conceptual design would be developed in collaboration with representatives of the Awabakal and Worimi communities.

#### Legend

- 01 Feature Entrance to Cultural Walk
- 02 Geological Rock Feature Paving
- 03 Dune Landscape Interpretation
- 04 Meeting/Ceremony Place
- 05 WSUD Pond
- 06 Plank Pathway Interpretation
- 07 Awabakal Interpretive Garden
- 08 Frog Pond and WSUD
- 09 Boardwalk
- 10 Amphitheatre Meeting Place





# 06 Masterplan

## Dune Landscape Interpretation

### Legend

- 01 Clipped Coastal Plants
- 02 Indigenous Garden
- 03 Dune topography/lawns
- 04 Informal Pathway
- 05 Native Shade Trees
- 06 Seating
- 07 Passive Lawn Spaces





03

04

05

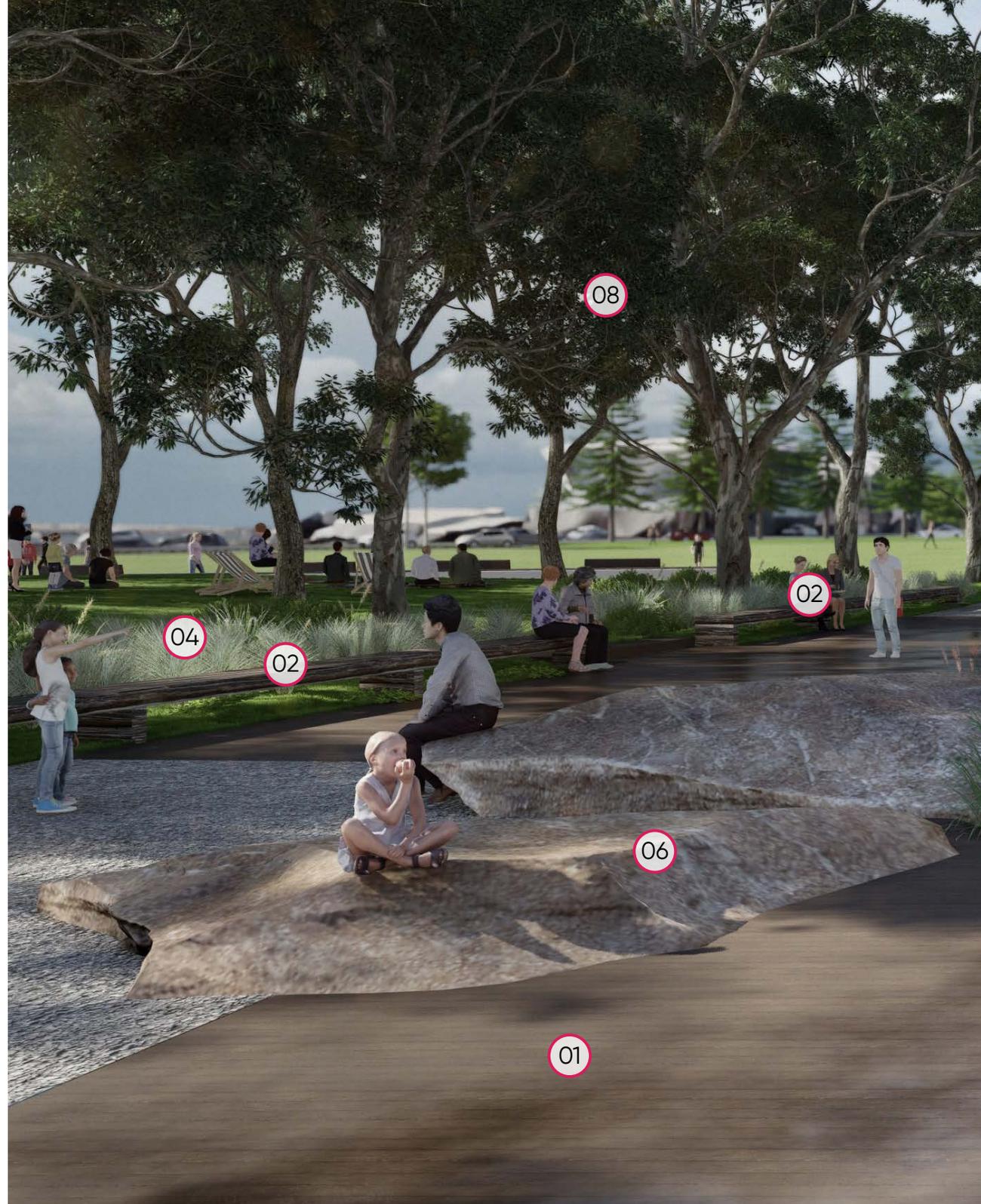
07

## 06 Masterplan

### Frog Pond

#### Legend

- 01 Boardwalk
- 02 Seating
- 03 Interpretive Signage
- 04 Native Garden
- 05 Riparian Vegetation
- 06 Rock Plinths for Seating
- 07 WSUD Treatment
- 08 Coastal Shade Trees
- 09 Shortland Lawn





09

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07

06

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## 06 Masterplan

### Variety Livvi's Place Playspace

The proposed play space has been planned in association with Variety the Children's Charity.

It is anticipated that the playspace will be an iconic centrepiece for the park and a regional destination.

Importantly it shall be a flagship all inclusive playspace that creates a thriving community hub for intergenerational community building and be of a quality and scale that is commensurate with the role of the park and its importance to the City.

The playspace is located in the western end off Foreshore Park, bridging between the Railway Carriage shed to within close proximity to Customs Plaza, and Watt Street and is well serviced by adjacent parking and potential bus drop off areas.

The playspace has been developed to be fun, inclusive, varied, surprising and to convey distinct Newcastle stories under the banners of trade, exchange, history, culture and environment.

Play opportunities include a junior and toddler space based around themes of the beach as well as a water splash pad with interactive water play, tug boats Awabakal canoes and water mazes.

In the centre of the play area is an iconic elevated play structure for the adventurous playfully using industry as its design cue, with a large swing set based on maritime navigation as well as a flying foxes.

Gardens and trees provide nature play and active climbing experiences thought the play space and along with purpose built shade structures provide important shade and cooling.

The play space will be fully fenced and is connected via a series of accessible paths, generous seating, gardens and lawns.

The play space will be fully fenced and is intended to cater for all ages and visitors with generous seating and shade areas for both those coming to support the active participants and the active play people too. There are also a variety of less active spaces for picnics or to just and relax.



Playspace Thematics and Precedents

(Top-Bottom L-R, JMD Design, TCL, Hilldistrictmums.com.au, Turf Design Studio and Environmental Partnership, TCL, JMD Design, 2021)

**Legend**

- 01 Entry Gateway
- 02 Kiosk and Amenities
- 03 Industrial Play Towers with Interactive, Sensory & Social Play
- 04 Double Flying Fox with Inclusive Seat
- 05 Tree Top Boardwalk
- 06 Trampolines
- 07 Quiet Space
- 08 Awabakal & Worimi Area
- 09 Whale Bone Giant Swing Set
- 10 Bark Canoes
- 11 Picnic Lawn
- 12 Coastal Water Play Area
- 13 Sea Grass Maze
- 14 Tug Boats
- 15 Sand Dunes Area
- 16 Accessible Sand Dune Mound
- 17 Nature-Based Play



🕒 Playspace (Plan) Not to scale

(TCL, 2022). \*Note: Playground concept is subject to change through the Detailed Design process.

## 06 Masterplan

### Industrial Play

Playing at Height • Flying Fox  
 • Climbing, Sliding • Active  
 Play • Auditory Play • Sensory,  
 Interactive Play • Bouncing •  
 Spinning, Social Play

#### Legend

- 01 Nut & Bolt Towers with interactive, sensory and social play
- 02 Inclusive Slide
- 03 Treetop Walkway to 1st level of play structure
- 04 Wheelchair seesaw
- 05 Wheelchair Carousel
- 06 Edge Seating to Garden Bed



Industrial Play (Concept Section) Scale NTS  
 (TCL, 2022) \*Note: Playground concept is subject to change through the Detailed Design process.



# 06 Masterplan

## Coastal Waterplay Area

Bucket • Inclusive • Interactive, Social Play • Educational • Jets • Water Curtain • Canoe Interps • Surprises • Sprays • Patterned, Textured and Accessible Surfaces • Wet Wheelchairs (located at kiosk)

### Legend

- 01 Sea Grass Maze
- 02 Accessible Tug Boat with Water Cannons
- 03 Elevated Tug Boat with Bucket
- 04 Bark Canoes
- 05 Seating Edge
- 06 Picnic Lawn





# 06 Masterplan

## Sand Dunes

Climbing, Sliding • Tunnels  
 • Social Play • Balancing •  
 Imagination, Education •  
 Discovery • Tactile • Sensory •  
 Swinging • Bouncing

### Legend

- 01 Covered Refuge/Shaded Space (connected to Kiosk)
- 02 Accessible Sand Dune Mound
- 03 Natue-Based Play
- 04 Sandstone Rock



01

02

03

04



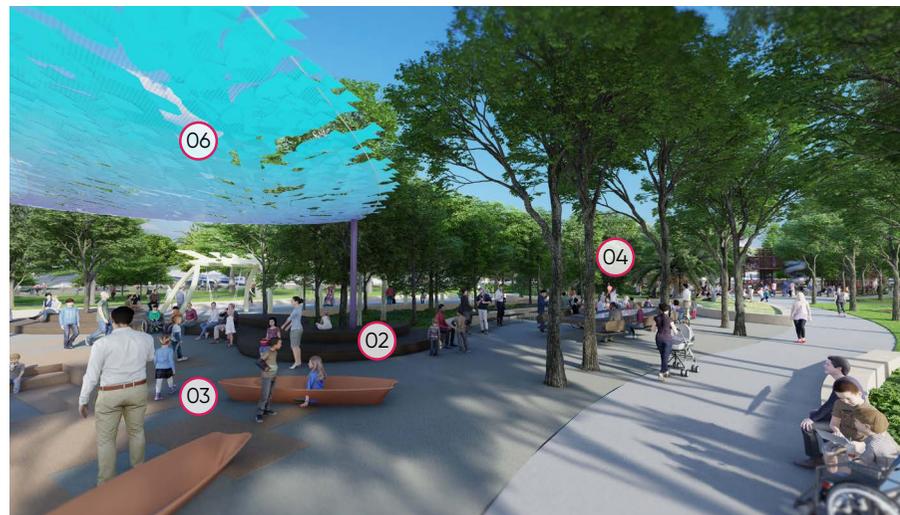
# 06 Masterplan

## Awabakal & Worimi Area

Gathering • Fishing Nets • Bark Canoes • Giant Swing Set • Beached Whale Story • Sea Fern Sculptures • Learning • Discovery

### Legend

- 01 Whale Bone Giant Swing Set with inclusive, standard and basket swing
- 02 Terrace & Edge Seating
- 03 Bark Canoes
- 04 Shady Picnic Area
- 05 Community Table
- 06 Shade Structure



### Kiosk and Amenities

There is an al fresco cafe and public amenities pavilion proposed adjacent to the play space and Community Commons.

The café shall reinforce the park as a community destination. Architecture to permeate existing and proposed landscape, providing appropriate amenity for the adjacent playspace as well as food and beverage for Community Commons and park users.

The building is to be modest in scale and provide appropriate shade provisions and be developed to the highest architectural quality. Public amenities are to meet the requirements of an all inclusive play space and surrounding parkland.

The concept design developed for the pavilion forms a collection of concave shapes responding to adjacent activities. With three public spaces carved out of the cylindrical building. The cylindrical form reuses the Newcastle colonial circle overlay that identifies important nodes, whilst simultaneously enhancing the node with three welcoming public spaces.

### Legend

- 01 Community Table
- 02 Water Collection Point (above)
- 03 Kiosk
- 04 Changing Place & Toilets
- 05 Covered Seating Area
- 06 Drinking Fountains



## 06 Masterplan

### Railway Carriage Shed

The Railway Carriage shed is an important heritage building and undercover recreational park space.

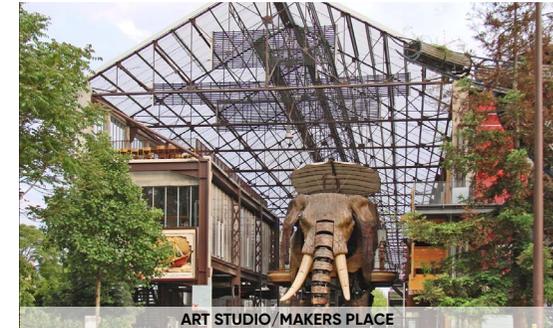
In association with the proposed community commons, cultural walk, play space and al fresco café, the Railway Carriage Shed will become a central hub for Foreshore Park providing a range of seating, bbq, and event spaces for the community.

The large undercover space will be improved with upgraded pavements, planting, furnishings and lighting and has further potential to be activated with pop events, programmed activities and community functions.

The images (left) illustrate the potential changing and temporary programs and activities that could be considered such as bouldering walls, art markets, pop-up food and wine events and botanical insertions.



MARKET SPACE



ART STUDIO / MAKERS PLACE



SENIOR PLAY ZONE



CONSERVATORY



CAFE



SHELTERED COMMUNITY SPACE

## 06 Masterplan

### The Groves

The Groves are an opportunity for shaded space and botanical display within Foreshore Park.

It comprises large canopy trees at close spacings to provide shade for informal gatherings, refuges for events, play and picnics.

They are located on either side of the Community Commons, creating a green buffer between park spaces.

#### Legend

- 01 Large Canopy Trees with lawn underneath
- 02 Footpath
- 03 Seating Edge (removable)
- 04 Community Commons
- 05 Shortland Lawn



## 06 Masterplan

### Community Commons

The Community Commons is a large unencumbered lawn space, suitable for events and passive recreation. It is flanked on either side of paths and shade trees and opens out to views of the Harbour.

Two large groves on either side of the Commons provide shaded seated areas and picnic spaces.

#### Legend

- 01 Tree Lined Footpath
- 02 Community Commons Lawn
- 03 Cultural Walk
- 04 Railway Carriage Shed
- 05 Amenities Pavilion and Cafe
- 06 Council Depot
- 07 Sandhills Community Garden
- 08 The Groves
- 09 Shortland Lawn



🕒 Community Commons (Plan) Scale 1:1000 @A3 (TCL, 2021)



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## 06 Masterplan

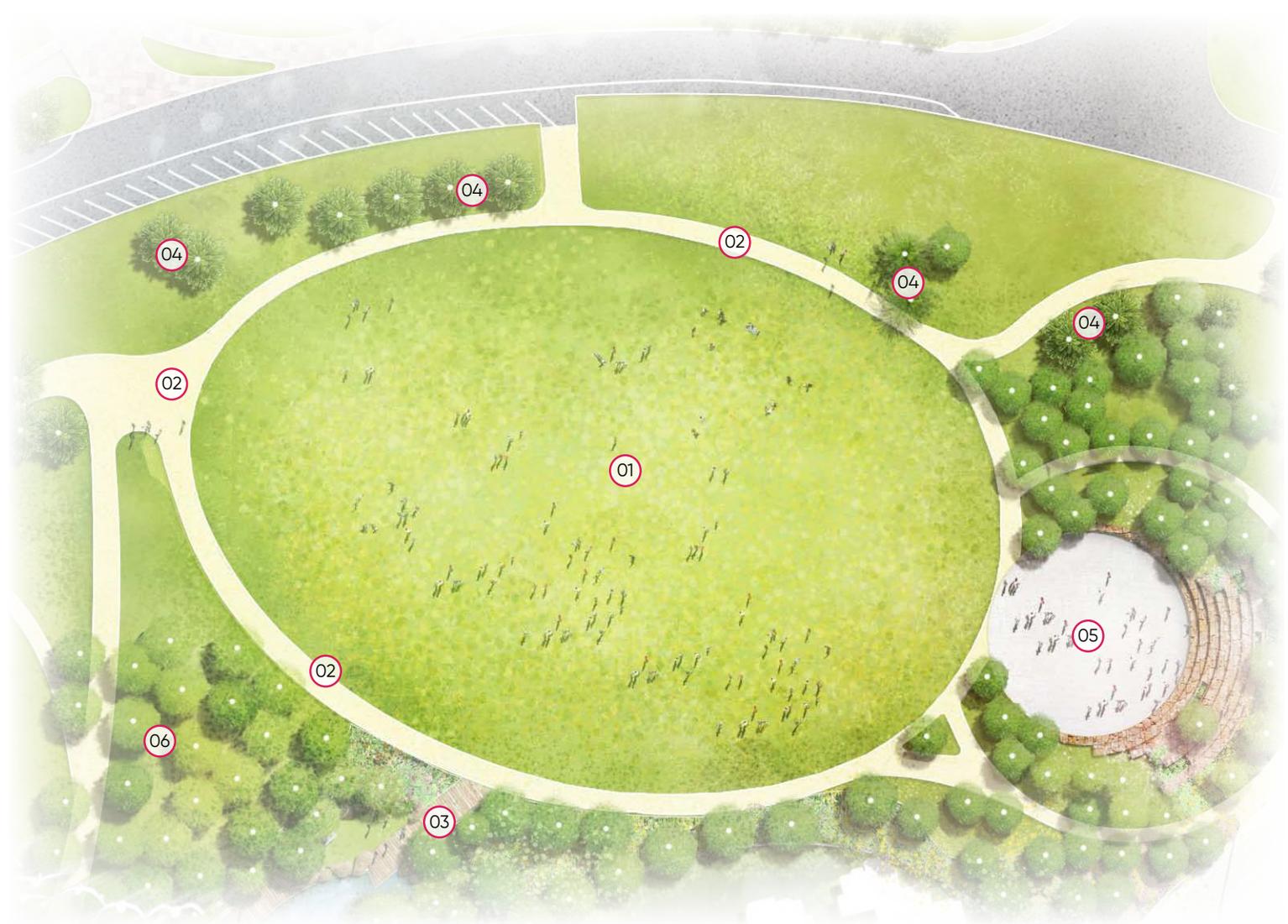
### Shortland Lawn Event Space

This great open lawn will be bordered by shady trees with clear views out to the harbour.

This area is intended to function as an everyday park space, encouraging passive park uses whilst also accommodating premier events for Newcastle.

#### Legend

- 01 Large Event Lawn
- 02 Footpath
- 03 Cultural Walk
- 04 Existing Pines
- 05 Amphitheatre
- 06 The Groves



Shortland Lawn (Plan)  
(TCL, 2021)

Scale 1:1000 @A3



## 06 Masterplan

### Amphitheatre

The Amphitheatre references the original sandhills - windblown sand that capped the hills between the Harbour and Newcastle Beach.

The existing amphitheatre area will be developed to encourage additional event uses as well as day to day activities such as roller-skating and scooter use.

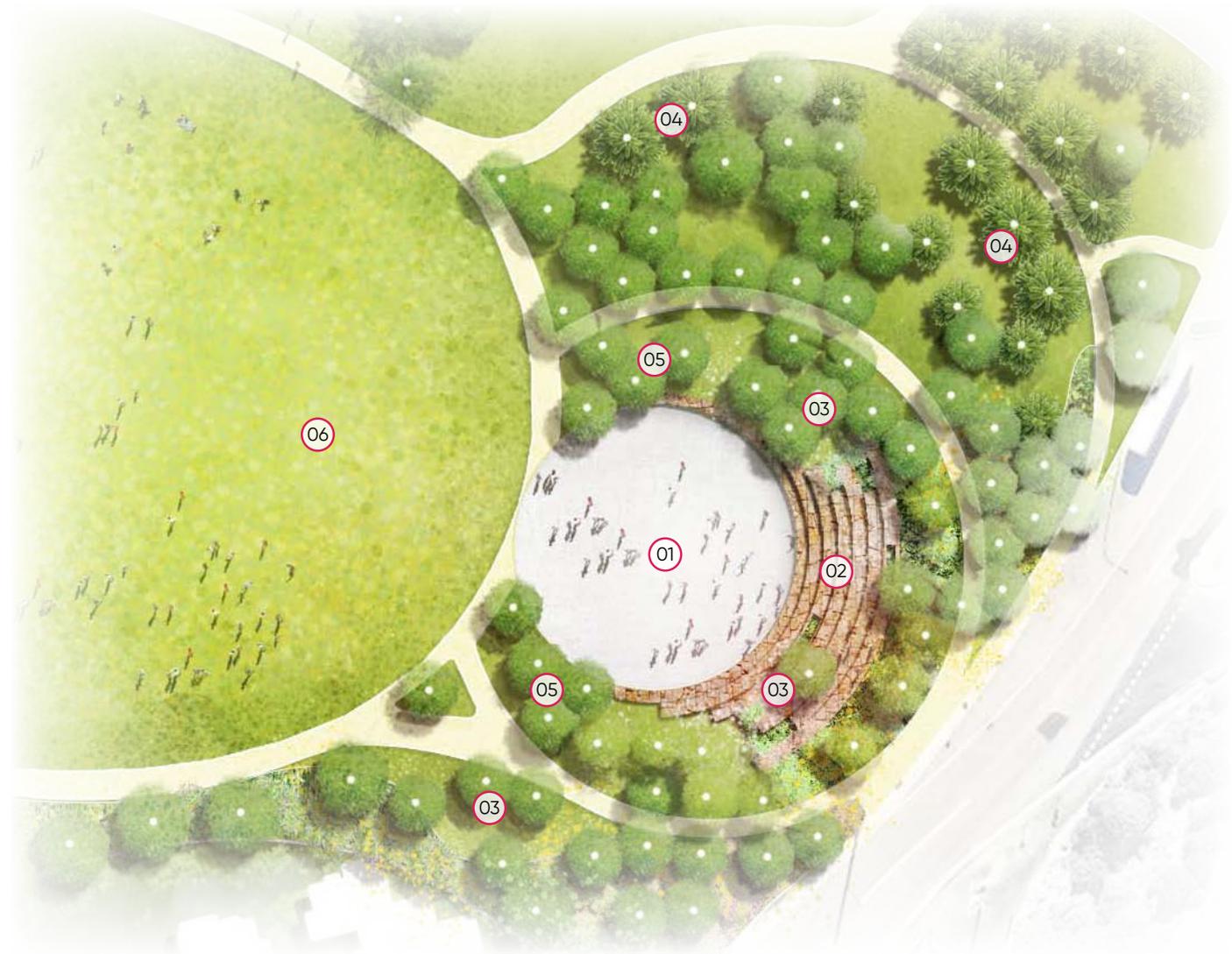
The central lawn will be paved to allow for scooter and rollerskating use as well as for staging events.

Tiered sandstone seating framed by indigenous gardens and trees will be provided in the centre of the space to reference the local geology and provide informal seating.

On either side of the tiered seating lawns will be maintained around the amphitheatre with additional shade trees.

#### Legend

- 01 Paved Amphitheatre
- 02 Sandstone Terraces
- 03 Indigenous Garden Planting
- 04 Existing Trees
- 05 Proposed Coastal Shade Trees
- 06 Shortland Lawn



1 Amphitheatre (Plan)  
(TCL, 2021)

Scale 1:750 @A3



## 06 Masterplan

### Amphitheatre

#### Legend

- 01 Paved Amphitheatre
- 02 Sandstone Terraces
- 03 Indigenous Garden Planting
- 04 Existing Pines
- 05 Existing Footpath
- 06 Shortland Lawn
- 07 Proposed Coastal Shade Trees



# 06 Masterplan

## Amphitheatre

### Legend

- 01 Paved Amphitheatre
- 02 Sandstone Terraces
- 03 Shortland Lawn
- 04 Footpath
- 05 Indigenous Garden Planting
- 06 Proposed Coastal Shade Trees





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## 06 Masterplan

### Community Gardens

Existing Sandhills community gardens to remain in their current location and linked into the proposed circulation of the Railway Carriage Shed and cultural walk.

### Landscape Trees and Gardens

Foreshore Park has limited tree diversity and canopy cover. The masterplan proposes to introduce additional trees to create a climate resilient park, reduce the impacts on urban heat island effects, provide beauty, shade, biodiversity and visitor enjoyment.

The desire for additional trees and shade was a key finding from the consultation process.

Trees shall be located in association with pathways to provide shade and balanced with open lawn areas allowing for event use.

Significant gardens and trees will be incorporated with the proposed play space to enframe the setting, provide shade as well as encourage nature play experiences.

Indigenous plantings and trees from the local coastal context and the broader Newcastle region will be used along and within the cultural walk to promote biodiversity, showcase indigenous flora, and provide a means to interpret Awabakal and Worimi stories.

Water sensitive initiatives are proposed along the cultural walk in wetlands near to the original frog pond.

The introduction of additional trees with a greater diversity of species will greatly



## 06 Masterplan

enhance the Foreshore Park experience.

Species shall be selected in part in how they contribute to the following criteria:

### **Botanical Diversity**

A broad representation of the botanical diversity of Newcastle's flora, including exotic, indigenous and endemic.

### **Climate Resilient**

Able to thrive in the sites challenging environmental conditions and selected in anticipation of future rising temperatures and extremes.

### **Shade and Amenity**

Contributing high amenity value for visitors, in form, shade, scale, flowers and seasonal display.

### **Spatial Experience**

Able to provide spatial enclosure and definition to reinforce path patterns, open lawns, clearings and groves.

### **Biodiversity**

Contribute to biodiversity values for the park

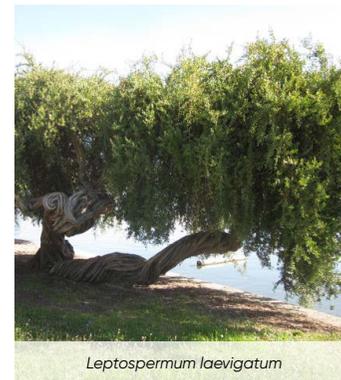
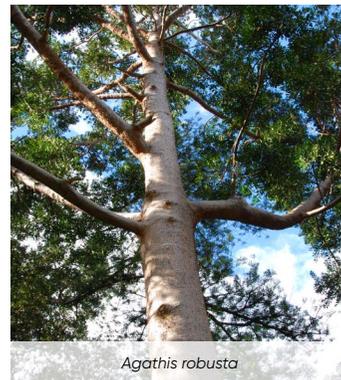
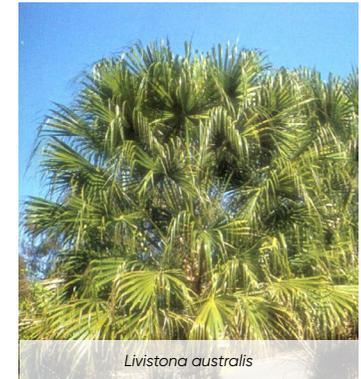
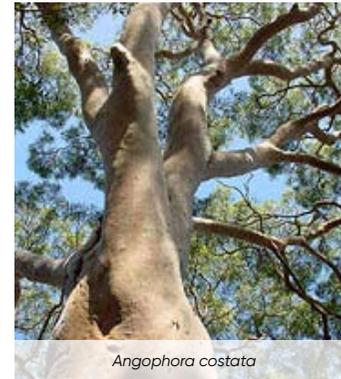
### **Ethnobotany**

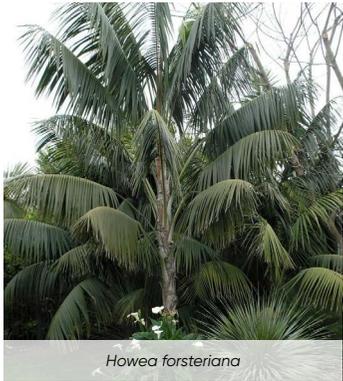
Linking the trees to the stories of the people who have lived in Newcastle.



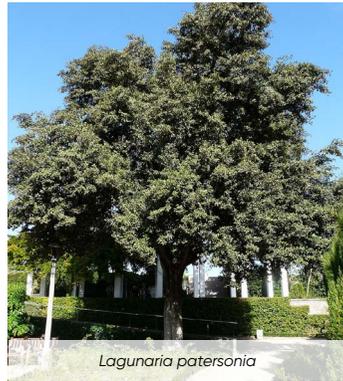
## 06 Masterplan

### Indicative Tree Palette

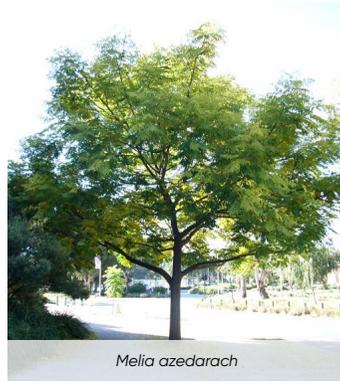




*Howea forsteriana*



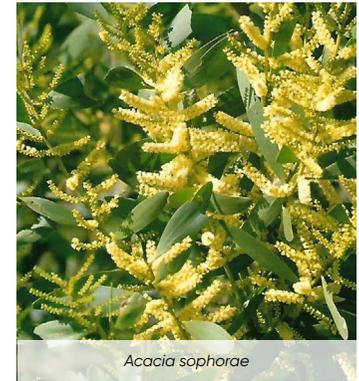
*Lagunaria patersonia*



*Melia azedarach*



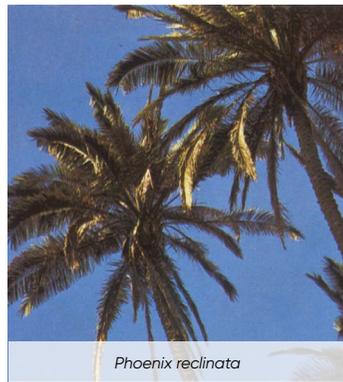
*Corymbia maculata*



*Acacia sophorae*



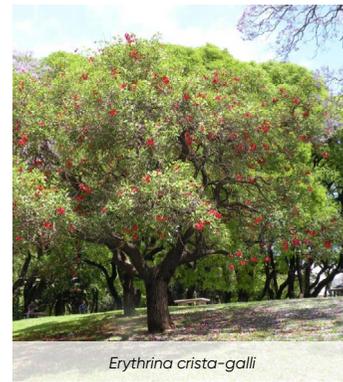
*Acacia saligna*



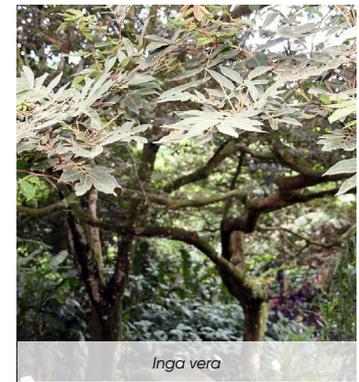
*Phoenix reclinata*



*Callistemon citrinus*



*Erythrina crista-galli*



*Inga vera*

## 06 Masterplan

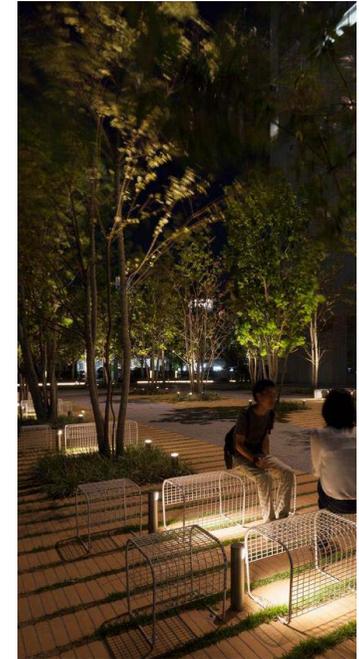
### Lighting

Lighting and night-time safety will be a key consideration for the ongoing design development and implementation for the masterplan.

A detailed lighting concept for the project site should be developed to ensure sites are appropriately illuminated, key features are highlighted, pathways are illuminated to best CPTED principles and utilize practice smart pole technology.

The lighting concept should seek to reinforce the masterplans principles of seamless connectivity along major circulation and cycling routes as well as interconnectivity to its city context.

Key nodes as defined in the masterplan should be highlighted with bespoke lighting solutions that reinforce the distinct spatial and experiential qualities of the spaces and landscape as well as provide memorable visitor experiences.



Lighting Consideration Precedents  
(Clockwise L-R, *TCL*, *unknown*, *unknown*, *TCL*, 2021)



# 07 Appendix





[newcastle.nsw.gov.au](http://newcastle.nsw.gov.au)

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**CCL 27/09/2022 - PUBLIC EXHIBITION OF THE DRAFT HARBOUR  
FORESHORE MASTERPLAN**

**ITEM-71    Attachment B:**    CCL 27/09/22 - Phase 1 Engagement Report

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# Let's Play!



# Inclusion Through PLAY

EQUALITY | COMMUNITY | ACTION | JOY

Document Set ID: 7237448  
Version: 1, Version Date: 23/12/2021





# By delivering the elements of the Variety Kids Support Framework:

We believe that all kids deserve a fair go in life.

So we help kids with these challenges:



Sick



Disadvantaged



Special Needs

## GRANTS

We grant a range of things to provide practical help to kids and organisations in need.



Equipment Grants



Therapy Grants



Technology Grants



Program Grants



Supplies Grants



Sunshine Coach Grants

## PROGRAMS

We run programs to educate & empower kids in need.

JUST LIKE YOU



## SCHOLARSHIPS

We give scholarships to encourage the talents of kids in need.



## EXPERIENCES

We hold events to engage & bring joy to kids in need & their families.



# Why do we need inclusive playspaces?



## The Importance of Play

Play is the right of the child. Play is children's work and the way they learn about themselves, each other and the world. Learning through play is one of the most important ways children learn and develop.



## Building Community

An inclusive playspace is where children engage with the natural world around them in the context of community. Building community, connections to community, healthy living choices and an inclusive society through inclusive play.



## Access ≠ inclusion

A truly inclusive playspace requires it to be bespoke universal inclusive design. We need change the way society plays by going beyond access to create spaces where we can all play together.

# The Imagineer Workshops

Variety facilitated school consultations at Wallsend Primary School (60 students), Newcastle Senior School (15 students) and St Columba's Primary School (25 students).

We opened our consultations sharing thoughts and understandings of what inclusion meant to the students and what we needed to think about if we are to create an inclusive place at Foreshore Park.

Over 80% of all students had been to Foreshore Park

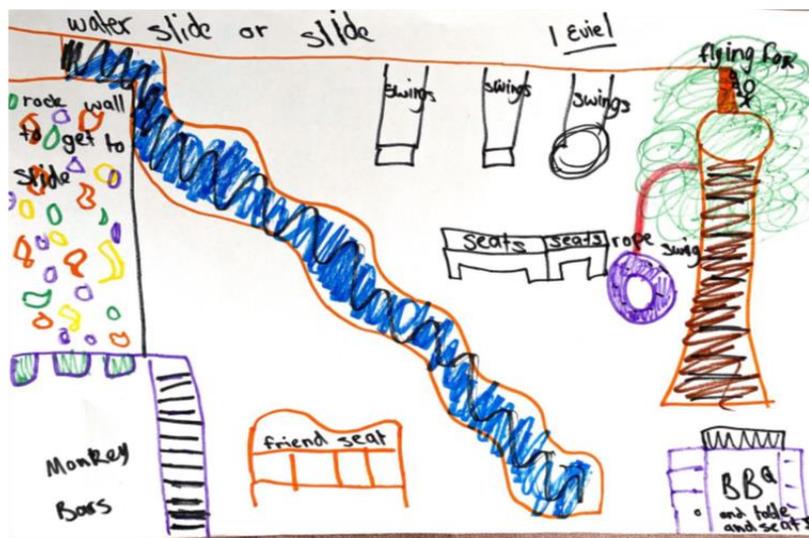
**“Inclusion is making people feel they belong”**



# Feedback – How the kids like to play

## What games we play

- Hide and Seek
- Racing and Chasing games
- Imagination games - Treasure hunts/hairdressers/cooking /shops/mums and dads/warriors
- Climbing and being high
- Ninja Warrior
- Sitting down and playing music
- Sardines (squash into spaces)
- Jump, fly and launch from high spaces and land
- Spies and secret code breakers



## How we like to play

Free Play – We like equipment that doesn't tell us how to play to what to do.

*"We like to imagine the equipment to be an anything we want it to be"*

Playing together

*"We like to play with our friends and make new friends"*

*"We like equipment we can get lots of people on and play together with it"*

*"Let's have a friendship seat"*

Physical Play – running and races and climbing and HIDE and SEEK. Wrestling, skate park, Parkor

*"We like tracks to run along and spaces to hide and squish in".*

*"We like to run around like we are going on an adventure and exploring"*

*"We want to climb really high, really high"*

*"We need a grass space to wrestle"*

Relaxing time - Comfort, sit ,chat and hang out. Listen to music do drawing or art, read

*"I like green space to be with nature"*

Family Play – we go to the park with our families so need spaces for little kids and BBQs and seats and tables.

# Feedback – What kids would LOVE in the playspace

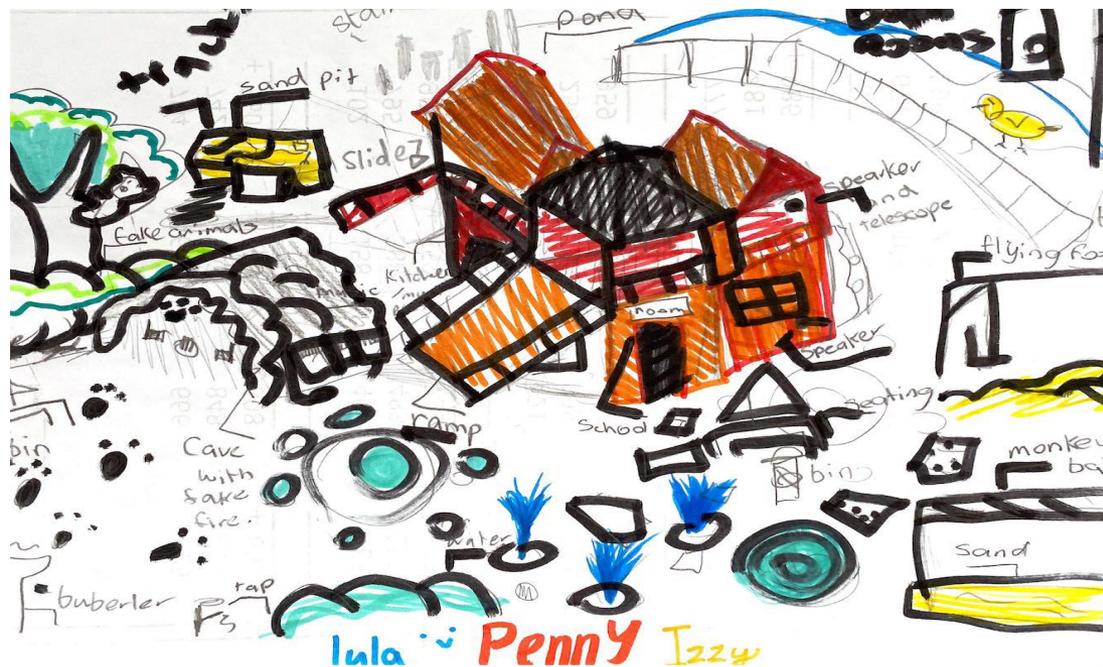
Tunnels and bridges and a track to explore and run around  
 Little caves, rooms, spaces we can squish into, spaces we can hide.  
 Sandpit with hidden treasure we can discover

Things we can move that make other things happen – Cause and effect  
 Lots of TREES - Lots of real trees for shade and to climb and look out from

A tree house with a slide, places to hide, bridges, shelves and rooms inside, space to be together, toys to play with

A green space where we can relax  
 and be with nature and plants  
 A garden we can go in - a hedge maze  
 A green hill to sit on and roll down  
 Soft space to wrestle on

A relaxing space we can chill out  
 Where listen to music and hang out  
 Where we can dance  
 Where we can read  
 Where we can draw and do art  
 A place where we can hang with our friends



Indigenous artwork in the space.

# Feedback – What we would LOVE in water playspace

## “Make it a Beach there”

A big fountain that spurts up

A water slide or slide we can splash in the water

A water maze

A waterfall – flowing water

Fish in a pond to feed

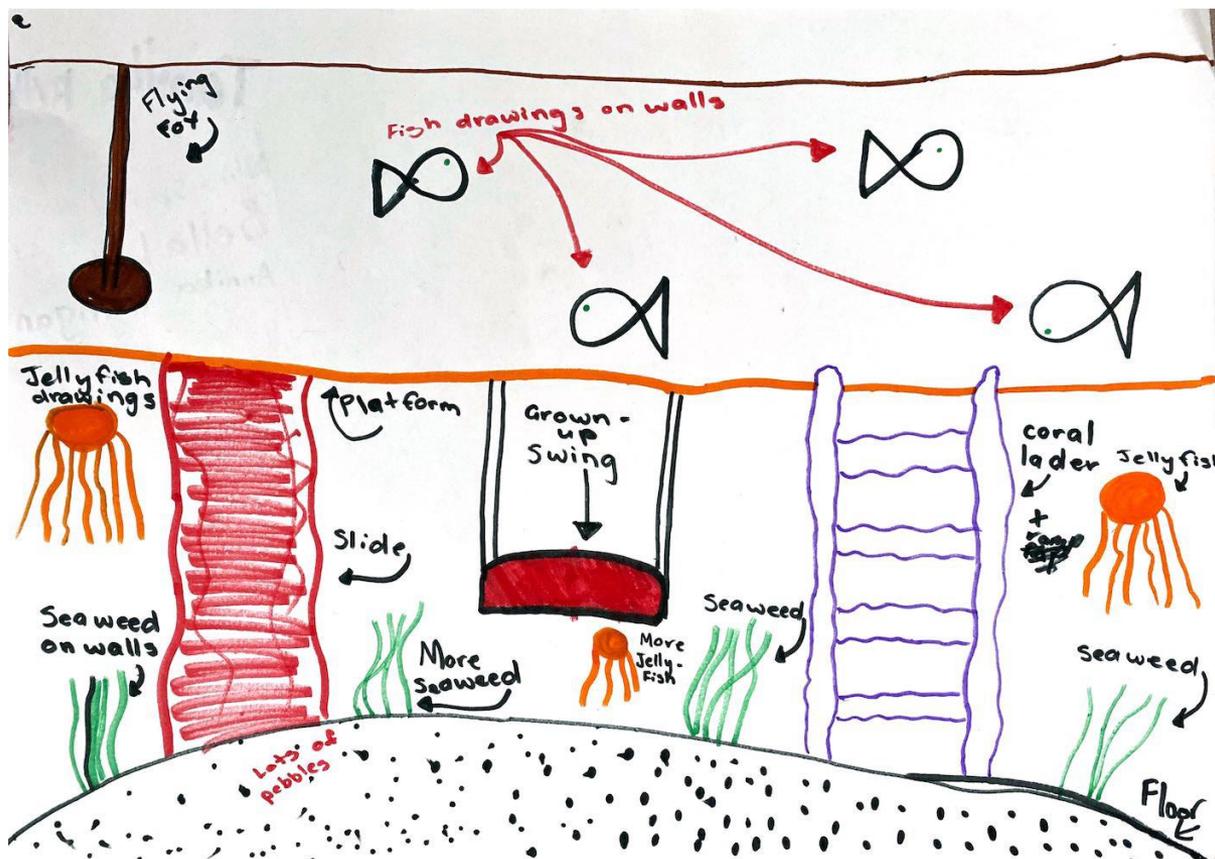
Bridges

A big ship or a boat

A dolphin swing

Underwater animals - fish, crocodiles and sea creatures

Water, water, water – Buckets to dump/ Pumps/ trickling water/water sprays tubes that show how it flows



# Recommendations

All consultations heavily focused on:

- It's a social place where we all play together and go with our families and friends
- Connection to water – making it like a beach, focus on boats and fish and sealife
- Use of natural resources – lots of trees for shade and to play in and on, gardens to explore, tree houses, caves
- High physical play value (for multigenerational play and recreation)
- Being connected to the wider park for big games like soccer, footy, skatepark, bike and scooter tracks.
- Adventure and exploration where we develop imagination
- Risk progression and universal design principals very important



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**CCL 27/09/2022 - PUBLIC EXHIBITION OF THE DRAFT HARBOUR  
FORESHORE MASTERPLAN**

**ITEM-71**     **Attachment C:**     CCL 27/09/22 - Phase 2 Engagement Report  
Summary

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# Newcastle Harbour Foreshore Masterplan

## Phase 2 community engagement Snapshot report



We're developing a masterplan to guide the future of Newcastle Harbour Foreshore.

Community engagement activities were carried out from 16 February to 5 March 2021 to gain feedback on preliminary concept designs for Foreshore Park, including a new inclusive play space being developed with our partners, Variety – the Children's Charity.



## Engagement activities – Key findings



**Have Your Say webpage**  
2,911 visits  
244 online polls completed

**77%**

said the preliminary concepts looked great or OK



**Social media**  
Post on CN pages & large media outlets

**1000+ 'likes' & 'loves'**

across two CN posts and two posts from large local media outlets



**Social Pinpoint**  
256 comments or ideas received on interactive maps

### People would like to see more:

- Trees and shade
- Facilities for skating and scooting

### Improvements to consider:

- Switch location of playspace with Picnic/BBQ Lawn
- Move café to preserve view lines from stairs to the harbour
- Repurpose frog pond for skating and scooting
- Retain existing community garden



## Submissions

31 written submissions

## Key issues raised in submissions:

- playspace too close to residents and businesses, with associated noise impacts and loss of passive recreation
- inadequate parking and increased traffic
- amount of event space
- calls to retain the existing community garden
- preference to retain frog pond as a water feature



## Drop-in information sessions

250+ attendees across three sessions

Feedback at two of the sessions was positive with a broad cross-section of the community saying the project will be great for our city.

Local residents raised concerns around:

- location of the playspace
- usage of the Rail Shed & amenities
- existing community garden
- location of the proposed cafe impeding sight lines
- traffic and parking, including current 'hoon' activity, and
- inadequate consultation.



## Survey

194 surveys completed about walking and cycling on Wharf Road



Opinion divided on whether the 30km/h on Wharf Road should be retained



Most would like to see an additional off-road cycle friendly path along Wharf Road.

## Next Steps

Thank you to everyone who provided feedback. The information is being reviewed and considered by the project team.

The Harbour Foreshore community reference group (CRG) will discuss the Phase 2 engagement findings and any revisions to the concepts that will form the draft Masterplan.

The draft Masterplan will be placed on public exhibition for further community feedback, before going to Council for adoption.



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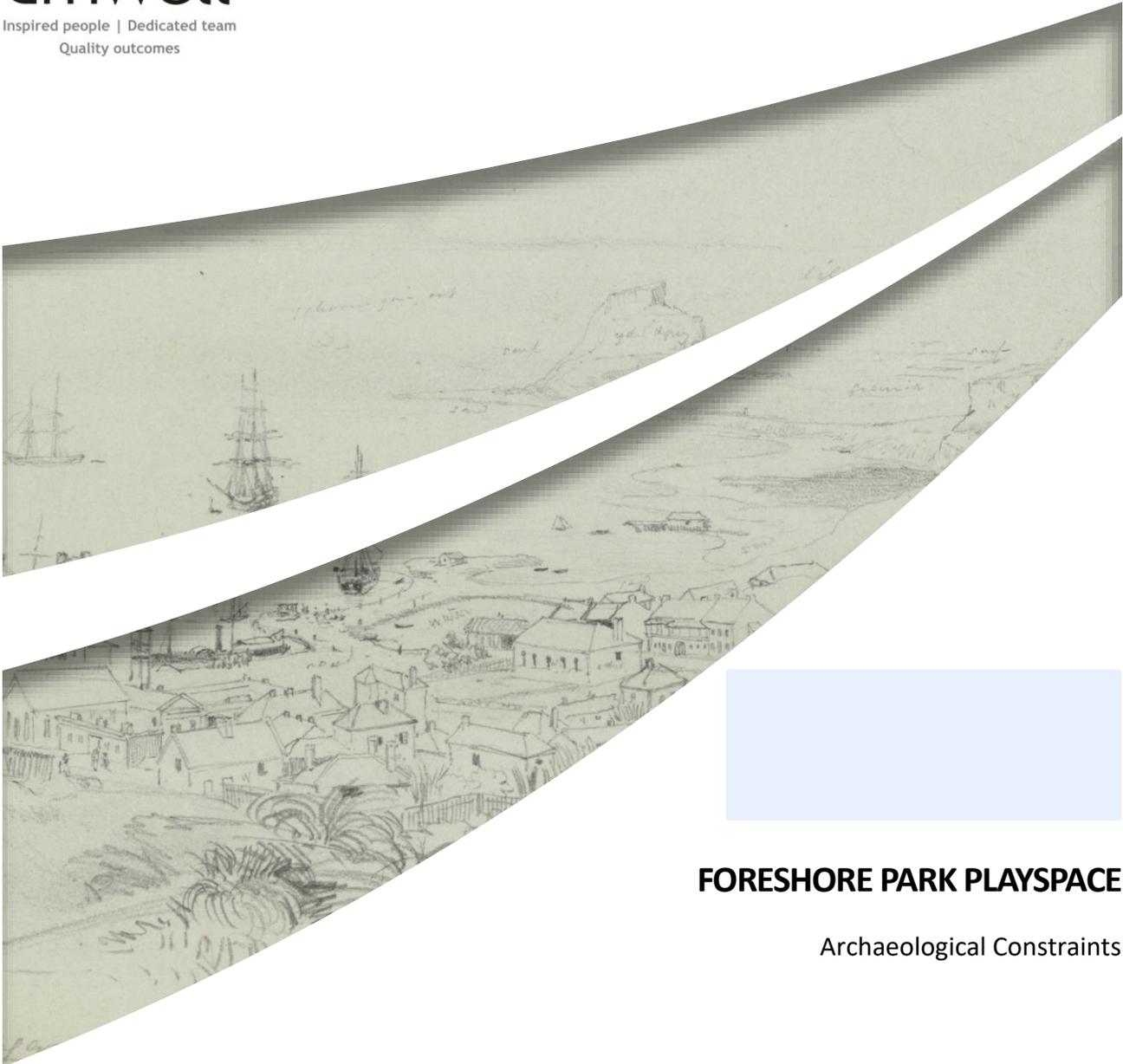
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**CCL 27/09/2022 - PUBLIC EXHIBITION OF THE DRAFT HARBOUR  
FORESHORE MASTERPLAN**

**ITEM-71**     **Attachment D:**     CCL 27/09/22 - Foreshore Park Playspace  
Archaeological Constraints Analysis

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**FORESHORE PARK PLAYSPACE**

Archaeological Constraints

**DRAFT**

June 2022



## FORESHORE PARK PLAYSPACE

Archaeological Constraints

### DRAFT

Prepared by  
Umwelt (Australia) Pty Limited  
on behalf of  
City of Newcastle

Project Director: [Project Director]  
Project Manager: [Project Manager]  
Technical Director: [Technical Director]  
Technical Manager: [Technical Manager]  
Report No. [Report No.]  
Date: June 2022



[Add Addresses here if required]



QMS Certification Services

This report was prepared using  
Umwelt's ISO 9001 certified  
Quality Management System.

**Acknowledgement of Country**

*Umwelt would like to acknowledge the traditional custodians of the country on which we work and pay respect to their cultural heritage, beliefs, and continuing relationship with the land. We pay our respect to the Elders – past, present, and future.*

**Cover Image: Detail of Newcastle, NSW 1853 sketch by G.F. Angas**

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Rev No.	Reviewer		Approved for Issue	
	Name	Date	Name	Date

Add Executive Summary and/or Abbreviation heading here via the 'Exec Summ, Abbrev headings' button on the Umwelt Tools tab, or delete here TWICE if not required.

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# 1.0 Introduction

Umwelt has been commissioned to prepare a preliminary archaeological assessment for the City of Newcastle’s (CoN) proposed Foreshore Park Playspace project (the project) (refer to **Figures 1.1** and **1.2**).

This assessment comprises an:

- Aboriginal due diligence assessment
- Preliminary historical (non-Aboriginal) archaeological assessment.

## 1.1 The Foreshore Park Playspace Project

The project works proposed to be undertaken as part of the Foreshore Park Playspace project include:

- Sewer connection from proposed amenities to sewer (nominal 3 metres deep)
- Potential sewer relocation comprising directional drilling / under boring with excavation at junctions
- Screw piles to major structures (amenities, industrial climbing structure)
- Mass concrete block footings to swings and play equipment.

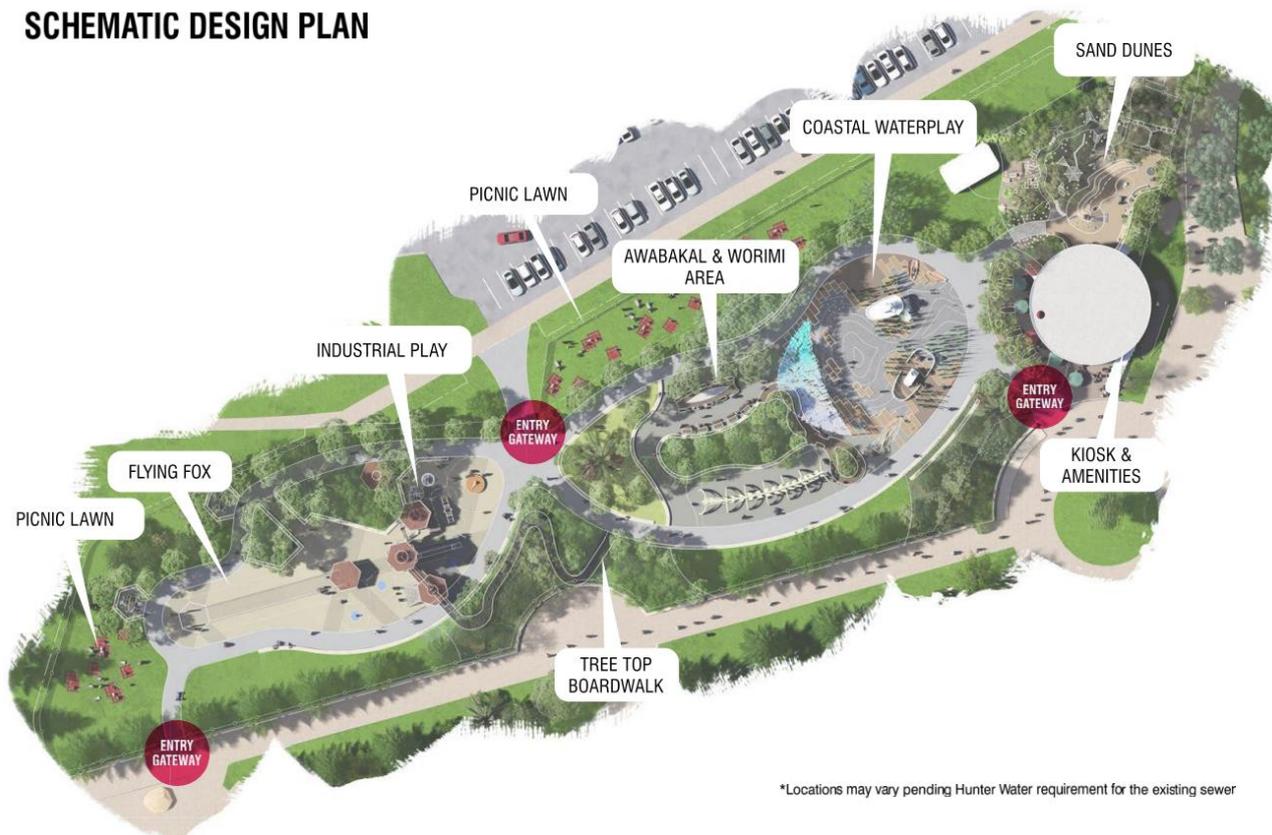


**Figure 1.1 The Project Area**

Figure shows the project area (outlined in red) within the western portion of Foreshore Park

Source: Nearmaps 2022 / CoN

## SCHEMATIC DESIGN PLAN



**Figure 1.2 Schematic Design Pan**

Source: T.C.L + Bosque

## 2.0 Aboriginal Heritage Information Management System

A search of the Aboriginal Heritage Information Management System (AHIMS) undertaken on 3 June 2022 identified no Aboriginal sites within the project area.

The AHIMS search result is provided as **Appendix 1**.

Four sites are identified in the general vicinity of the project area; between 50 and 180 metres to the south (refer to **Figure 2.1**):

- AHIMS # 38-4-1632: The TA1 Newcastle artefact scatter was originally recorded while undertaking historical excavations along Watt Street. As part of the excavations, an AHIP was sought, and the site was salvaged and is identified as 'destroyed' on the AHIMS register.
- AHIMS # 38-4-1695: The 11-15 Watt Street IF 1 is a recorded open artefact camp. However, on 13 March 2015 an AHIP was granted for the collection and salvage of the site as part of the re-development works. As such, this site has either been destroyed as part of an AHIP or is to be destroyed through salvage as part of the AHIP.
  - Note this site was the only site identified in the immediate vicinity of the project area (refer to AHIMS search result is provided as **Appendix 1**). The site was registered in the wrong location. **Figure 2.1** below indicates AHIMS # 38-4-1695 in the correct location at 11-15 Watt Street.
- AHIMS # 38-4-1020: Coutts Sailors Home PAD1 is located within the former convict lumber yard site along Scott Street, Lot 2 DP 706760. AHIMS advanced search data identifies that a permit has been issued for this PAD. It is unknown whether any further archaeological works have been undertaken as part of the permit issued for this PAD. It is noted however, that the AHIMS site 38-4-0957 (below) is located within the same Lot and DP as 38-4-1020.
- AHIMS # 38-4-0957: The NCL 931 site is identified as an open camp site and is located along Bond Street. It was noted that the assemblage was recovered from backfill deposits which dated from the 1987 excavations of the area. The AHIMS data indicates that a permit was issued for the site. It is unknown whether any further archaeological works have been undertaken as part of the permit issued for the site.



**Figure 2.1 AHIMS Registered sites**

Green shows sites registered as 'artefact'. Yellow indicates 'potential artefact deposit'.

Source: Nearmap (April 2022) Data source: AHIMS (2022), NSW DFSI (2021)

## 3.0 Archaeological Management Plans

### 3.1 Newcastle Archaeological Management Plan 1997

The Newcastle Archaeological Management Plan (NAMP – Suters Architects 1997) was prepared to give an indication of the nature and extent of historical archaeological resources in central Newcastle and to provide a framework to ensure historical archaeological resources are recognised and integrated into the urban planning framework.

The NAMP is a planning tool that provides an overview of areas that require the consideration of archaeological issues in conjunction with any development applications.

The NAMP identified nine archaeological precincts within Newcastle that define areas ‘in which a common pattern of development has occurred and may be expected to contain an archaeological resource of some cohesive characteristic’ (Suters Architects 1997:40). The project area is located in Precinct 8 Harbour Foreshore described as:

*That land north of Hunter Street being the original Harbour Foreshore. This includes the previously identified Honeysuckle Point, and that narrow strip of land controlled by the AA Company. Associated with the development of the railways, coal loading, wharfage and industry (Suters 1997).*

The project area is located partially within Indicative Archaeological Sites as identified in the NAMP. **Table 3.1** and **Figure 3.1** detail the Indicative Archaeological Sites the project area is partially located within.

**Table 3.1 NAMP Indicative Archaeological Sites**

NAMP Item No	Street Address	NAMP Occupation History	NAMP Archaeological Potential
1007 Boat Harbour	51 to 55 Wharf Road, Newcastle East	Boat harbour relocated to position near Market Street 1852.  Land reclamation east of Watt Street commenced during 1840s, and was boosted after the arrival of the Railway, which later extended into this area, with the extensive marshalling (goods) yards	Potential site
1198 Wharf Street Wharf and Boat Harbour	51 to 53 Church Street	Timber Wharf at Watt Street built 1804. Replaced 1833 by rounded-end stone wharf. Straight ‘Ballast Wharf’ added 1840.  Ballast Wharf extended 1863/1864 to become Kings/Queens Wharf.	Disturbed / Potential site Ballast Wharf, and some of Watt Street stonework, might survive under Wharf Road and Promenade. Rest of the boat harbour would be under west end of The Foreshore, but was unformed and not recognisable



**Table 3.2 AMP Review Inventory No. 2176285**

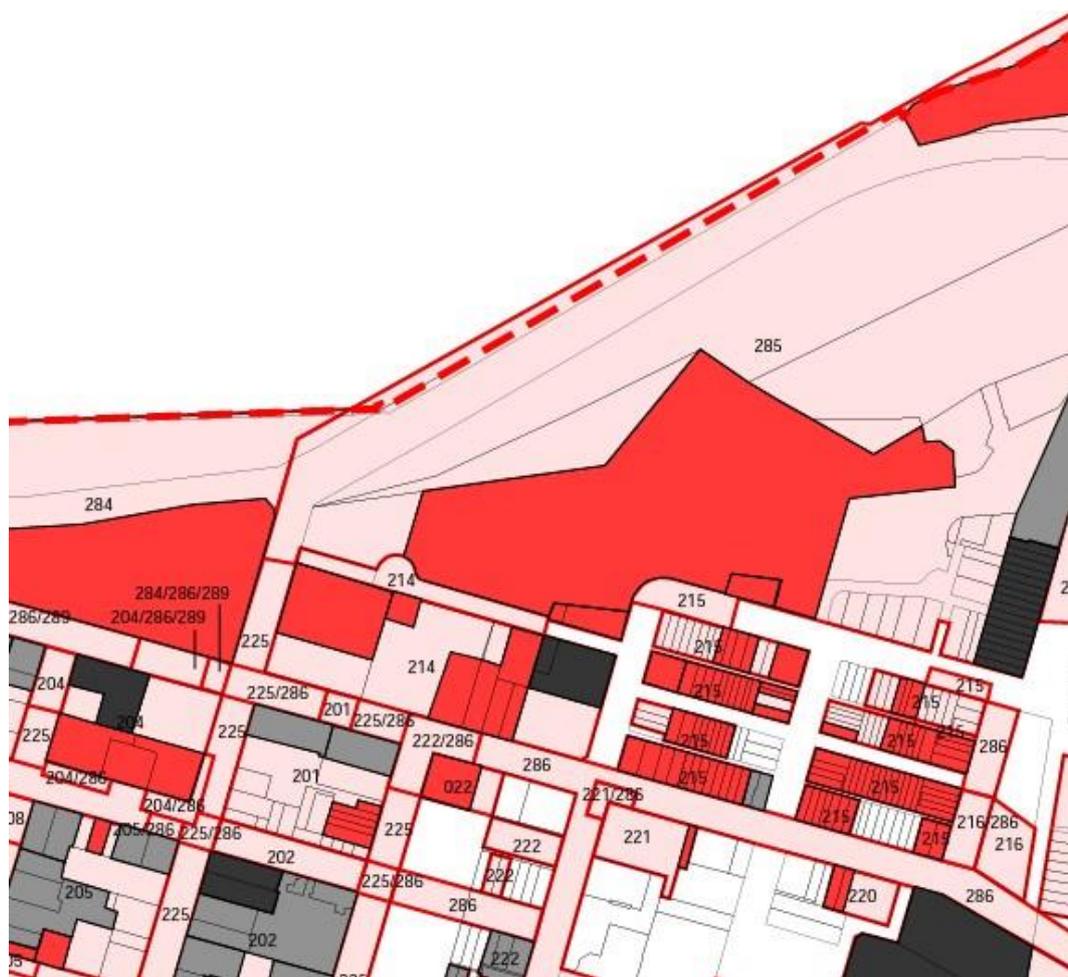
Penal Settlement 1801-1821	Town Development, 1820s-1853	Railway and Port Infrastructure
<p>'Initial 1801 settlement used mooring place near base of Watt Street. During second settlement a wooden pier / jetty constructed at foot of Watt Street. This in turn replaced by circular masonry wharf</p>	<p>The area of the foreshore, principally to the east of Pacific Street became known as the Sandhills, because of the problem of wind blown sand, which delayed development in the area.</p>	<p>Plans for reclamation of the harbour or Hunter River Foreshore are seen as early as 1843. A new wharf was constructed to the east of Watt Street and the Old Wharf. The plans to extend a ballast wall from the new wharf in an easterly direction to join the end of Macquarie Pier or the Southern Breakwater are also shown.</p> <p>By 1853, the old wharf is labelled Queens Wharf, while the new wharf is labelled Moriarty's Wharf, with a stonemasons shed upon it.</p> <p>By 1860 substantial progress had been made on reclamation, infilling around the old wharf and the boat harbour, both of which are now no longer shown on plans. There is a railway siding to the end of the wharf as constructed and a goods shed on a branch siding</p> <p>By 1874 the reclamation of the foreshore is largely completed, with wharfage and steam cranes. There are extensive railway sidings along the river frontage</p>



**Figure 3.2 Detail of Inventory of Archaeological Sites Mapping**

Project area is within AMP Review Inventory No. 2176285

Source: Higginbotham 2013



**Figure 3.3** Detail of Archaeological Potential Mapping

Project area is within AMP Review Inventory No. 2176285. Red indicates 'Standing buildings or parts of buildings' pink indicates 'little disturbance'

Source: Higginbotham 2013

## 4.0 Historical Context

### 4.1 Reclamation of Foreshore

The entire foreshore area on the southern side of the harbour was originally a series of mud flats and shoals, with the original shoreline running along what is now the southern side of Scott Street, crossing the Newcastle Railway Station and following the approximate line of Bond Street and Stephenson Place. A number of early plans and maps indicate the original location of the shoreline (refer to **Figure 4.1**).

The first modification of the shoreline was the erection of the government wharf at the foot of Watt Street in approximately 1804. In 1832, as the harbour began to become busier with ships loading coal, work restarted on the breakwater between the shore and Nobby's Island, which although started in 1818 had been suspended in 1822 as convicts were relocated to Port Macquarie. The breakwater was finally completed in 1846, although ongoing repairs to breaches in the wall were required well into the 1870s. With the exception of the Watt Street wharf, Australian Agricultural Company staithes and the unfinished breakwater few other works were undertaken on the harbour until the 1840s.

In late 1841 a new Circular Wharf was started to the east of the Watt Street wharf. Built of masonry blocks shipped in from Sydney, the wharf was not robust enough to withstand the wash of the large ships moving up the harbour and was strengthened first by timber supports and then with the construction of a Ballast Wharf running parallel to the shoreline back to the Watt Street wharf. The effect was the creation of a wharf and a small boat harbour within which boats could shelter (refer to **Figures 4.2** and **4.3**).

In 1846 with the breakwater finally completed, the harbour became increasingly safer for ships. In June 1846 Newcastle was declared a Free Warehousing Port allowing goods and produce to be exported directly from Newcastle without having to go through Sydney. The following year when the Australian Agricultural Company's monopoly on mining in Newcastle came to an end and new mines began to open, the results of this and the free port were a dramatic increase in the amount of shipping sailing to and from Newcastle, and the need for better berthing facilities for them.

Despite this little was done to improve the harbour. A major concern was the depth of the channel, which was adequate at the entrance but quickly shallowed out once inside the harbour itself and around the available wharfage. Mud banks separated the main channel from the Australian Agricultural Company coal shutes, with small lighters needed to load ships of any size. In 1855 Edward Orpen Moriarty was appointed Engineer-Surveyor responsible for Hunter River Improvements. His plan was to control the sea entrances through extended breakwaters and the river channel through stone training walls.

Work began in 1856-57, with the former Circular Wharf demolished and a new wharf, Queens Wharf built on timber piles, with copper sheathing along the line of the Ballast Wharf from Watt Street. The front of the wharf was dredged to a depth of 20 feet (6m) and by 1860 500 feet (152m) had been completed. At the same time as Queens Wharf was being built, a new sea wall was also being constructed to the west between Watt Street and Perkins Street. The combined result of both sets of works was a large section of reclaimed land along the waterfront (refer to **Figure 4.4** to **Figure 4.5**).

## 4.2 Reclamation and the Great Northern Railway

The construction of the Queens Wharf increased the demands for the extension of the Great Northern Railway (GNR) to meet them. The GNR, running between Newcastle and East Maitland (the original station was to be at Tarro before the line was extended to Victoria Street, East Maitland) had been completed to its then terminus at Honeysuckle, to the west of the Australian Agricultural Company staites and yards in March 1857. Although the Chief-Engineer of Railways, Frank Whitton had wanted the line to extend to the wharves as early as 1854 when it was being planned, the railways of both the Australian Agricultural Company and Newcastle Coal and Copper Company blocked the route. Whitton argued that stopping the line short of the wharves defeated the point of the line itself, which was being built with the view of extending it through the Hunter Valley to enable the shipping of agricultural produce from inland as much as it was to transport passengers. Recognising the inconvenience of the original plan, a new contract was made to extend the railway closer to the centre of Newcastle and to join the wharves. Newcastle Station at the foot of Watt Street was opened in March 1858, with a branch line running to Queens Wharf. The necessity of bringing the railway into the town centre in 1858 and building of wharves parallel to the line completed the reclamation project along the foreshore of east Newcastle and allowed for the formation of Scott Street, construction of the railway and construction of Newcastle Station itself.

Much of the reclamation was carried out by the dumping of ballast from visiting ships, both domestic and international. Ballast was used to form a wall behind which sand and silt were pumped from harbour dredges to form newly reclaimed land. A correspondent to the newspaper in 1877 when commenting on the reclamations further along the harbour at Bullock Island noted that:

*We have geological specimens from every part of the world. The whole expanse of ground has been built up of ballast from the ships that come to our harbour. In one place we tread upon a layer of London flint, next a collection of stones from the Mediterranean, succeeded by a rock mound from Scandinavian Coasts, blue stone from Melbourne, green trap from New Zealand, limestone from Singapore, even sunburnt bricks and glazed uncouth carvings from China.*

By the 1880s much of the work of reclaiming the southern shore of the harbour was complete and the line of foreshore that exists to the present was fixed in place.

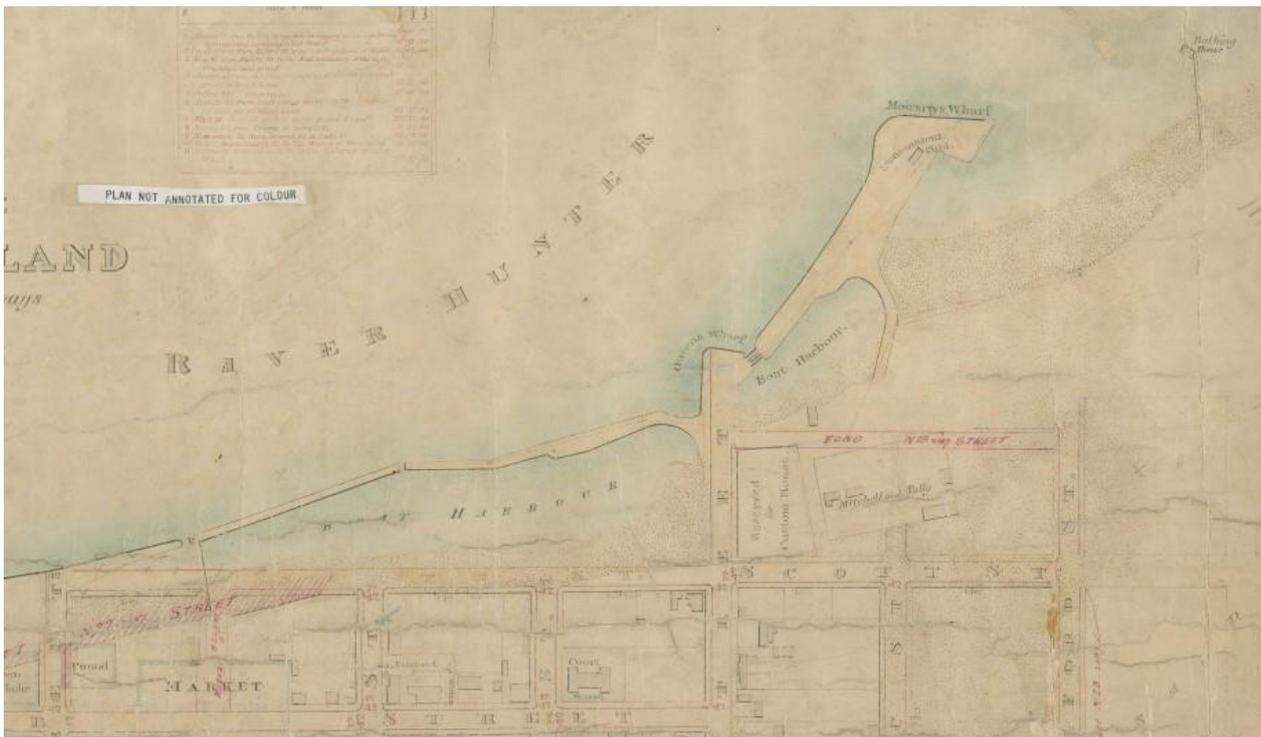






**Figure 4.3** Detail of Newcastle, NSW 1853 sketch by G.F. Angas  
Detail shows the boat harbour and ballast wharf to the east of Watt Street.

Source: National Library of Australia/Hunter Living Histories UoN



**Figure 4.4** Detail of c1858 Crown Plan survey of the foreshore  
 The survey shows the first wharf extensions, sea walls and boat harbours.

Source: LPI CP13-652



**Figure 4.5** Detail of Moriarty's 1858 Plan of the Harbour of Newcastle  
 The plan notes the old boat harbour to the east of Watt Street is 'to be filled in'.

Source: University of Newcastle Cultural Collections

## 4.3 Boat Harbours

In the years prior to the development of the railway or the connection of Newcastle to its surrounding hinterland via good roads, boats and small ships provided the main form of transport for people and goods up and down the Hunter River. Likewise, ships came and went from the harbour taking coal, timber and other agricultural products from the port. While wharves were being constructed from the 1840s for larger ships, the small boats working the river also required safe places to berth, load and unload. The provision of a safe harbour for these vessels was essential for the domestic economy of the Hunter region. A number of small boat harbours were subsequently built to service these lighter vessels as part of harbour reclamations and improvements made in the second half of the nineteenth century.

a new sea wall was also being constructed to the west between Watt Street and Perkins Street.

The boat harbour constructed to the west of Watt Street extended as far as Perkins Street (refer to **Figures 4.4** and **4.6**). A large sea wall was built to create the boat harbour, although the harbour was removed to make way for the extension of the Great Northern Railway in 1858 and replaced with the Market Street Boat Harbour.

### 4.3.1 Market Street Boat Harbour

With the loss of the boat harbour east of Watt Street and the reclamation of the temporary harbour west of Watt Street in 1858, another boat harbour was created along the foreshore opposite the Market Place, between Newcomen and Wolfe Streets. This new harbour, also known as the Market wharf basin, was formed by reserving an area during the filling of the temporary boat harbour that had extended to Perkins Street.

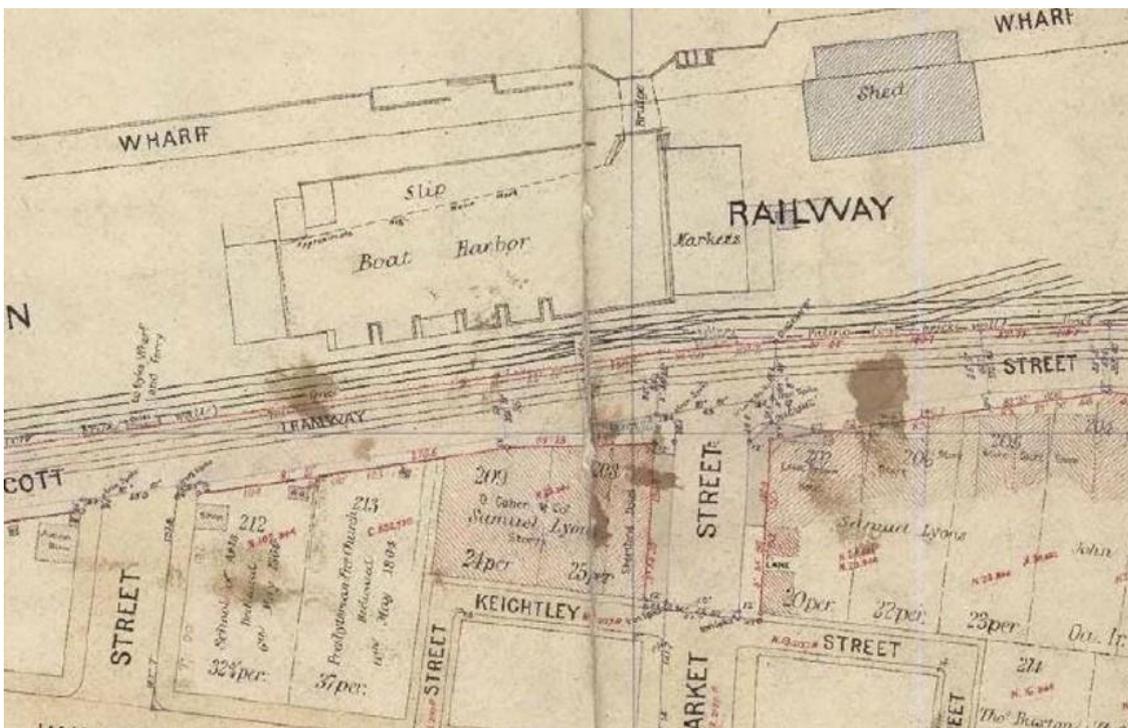
In August 1871 Newcastle Borough Council accepted plans for a large borough market building to be erected on the Market Reserve opposite the boat harbour. The announcement of the new market was preceded by the expansion, rather than the removal, of the market boat harbour. In 1881 the basin was recorded as providing wharf frontage of 510 feet (155 metres) of which 250 feet (76 metres) was reserved for farmers landing their produce for the market, 160 feet (49 metres) for use by watermen's boats and 100 feet (30 metres) for slips and sheds to house boats belonging to Government Departments (refer to **Figure 4.7**).



**Figure 4.6** Detail of 1854 Plan of Newcastle

The plan shows the boat harbour next to the Watt Street wharf. This was filled in by the late 1850s.

Source: Newcastle Regional Library



**Figure 4.7** Detail of Crown Plan 22-2137 showing the Market Street Boat Harbour

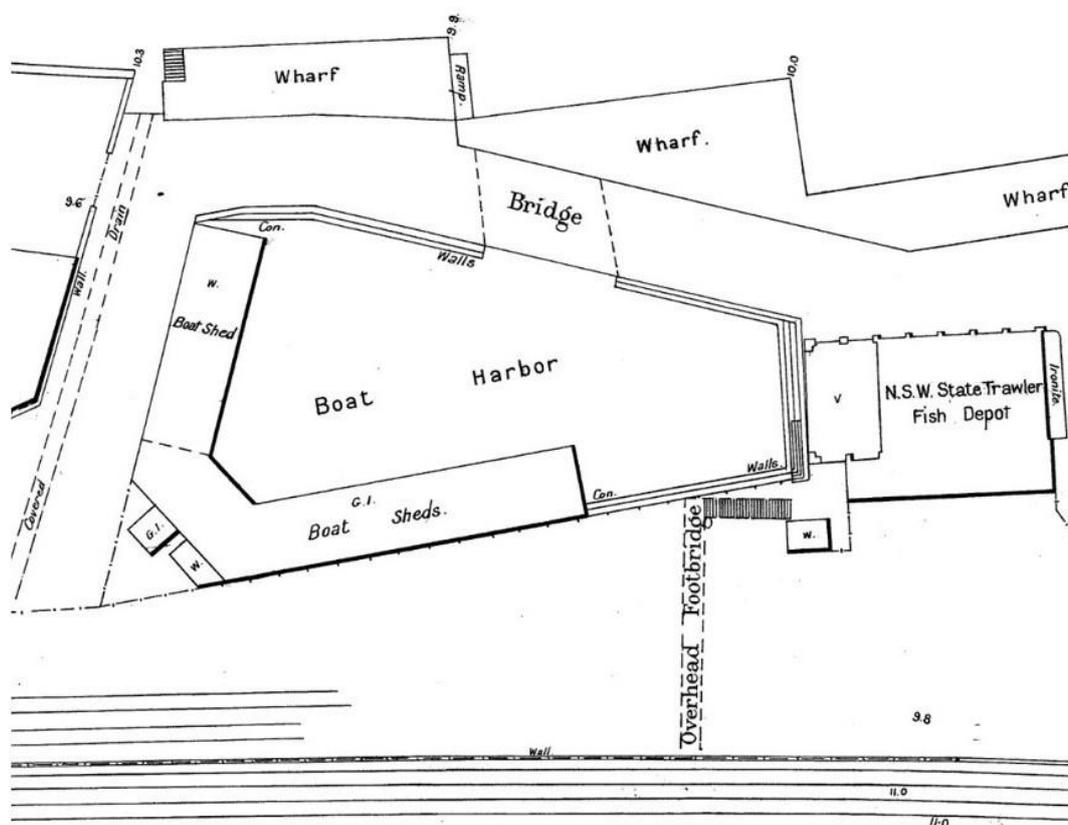
This plan shows the slipway on the northern side and the small finger wharves on the southern side. The market building is also shown on the eastern shore

Source: LPI

### 4.3.2 Perkins Street Boat Harbour

In 1899 work began on the construction of a new boat harbour close to Perkins Street. While work progressed slowly, the market wharf boat harbour continued to serve the river trade until 1901, when the railway commissioners began to plan for the expansion of the railway. Although the markets and boat harbour were managed by the Borough of Newcastle, the land was in the ownership of the railways. The bottom of the new Perkins Street harbour was lined with concrete blocks lowered in by crane, with the sides lined with sandstone, while the old harbour was filled in. Sheds from the old market were relocated to the new boat harbour.

The Perkins Street boat harbour continued to provide shelter to boats until it too was removed in 1960, with resumptions along the foreshore and the boat harbour replaced by a car park (refer to **Figure 4.8**).



**Figure 4.8 Newcastle and Suburbs Sheet 013 – City of Newcastle 1917-1923**

Image shows a detail of the Perkins Street Boat Harbour

Source: Newcastle Cultural Collections

## 4.4 Railyards

By the 1860s the area of reclamation of the Foreshore area from the end of Watt Street east towards the Breakwater (Macquarie Pier) had increased in size considerably with the Queens Wharf area at the end of Watt Street being resumed by this time. The Illustrated Sydney News and New South Wales Agriculturalist and Grazier (8 April 1875) reported that

*There was little improvement in wharf accommodation till the railway was commenced in 1855. In 1858, it became necessary to reclaim the land for the Railway terminus, and to lay down sidings for the shipment of coal. Sand was rolled down the space over which the tide flowed, up to the upper line of Scott-street, and was filled up to the Market Wharf.*

In the 1870s industrial rail workshops (including carriage sheds) were established at Newcastle (the Honeysuckle Point Locomotive Workshops) both to secure increased accommodation for the rolling stock used on the Great Northern line and in recognition of the facilities required to handle Newcastle's coal traffic. With no rail link to connect the Sydney and Newcastle rail systems until 1889 when the new bridge across the Hawkesbury from Brooklyn was constructed, the Honeysuckle Workshops were established separately from (but ran contemporaneously with) from Redfern/Eveleigh (Otto Cserhalmi + Partners P/L 2002).

As Newcastle Station and the wharf railyards to the east became increasingly busy additional carriage sheds were constructed to the east of the Honeysuckle Workshops. The 1880s saw rapid growth of the railway workshops in Newcastle in response to the expansion of the rail system, port and wharf; including increased coal (and other goods such as wool) and passenger traffic.

Extensive rail yards were established in the area of current Foreshore Park, in addition to the Zaara Street Power Station which was commissioned in 1915 and demolished in 1978 (refer to **Figures 4.9 to 4.13**).

The redevelopment of the rail yard and Power Station area into the current Harbour Foreshore Park was one of Newcastle's high profile projects for Australia's Bicentennial Celebration (1988). The Bicentennial Project extensively re-landscaped the entire Foreshore area, including the planting of 447 trees (Placemark 2017).



**Figure 4.9** Detail of 1874 Plan of the Government Railway

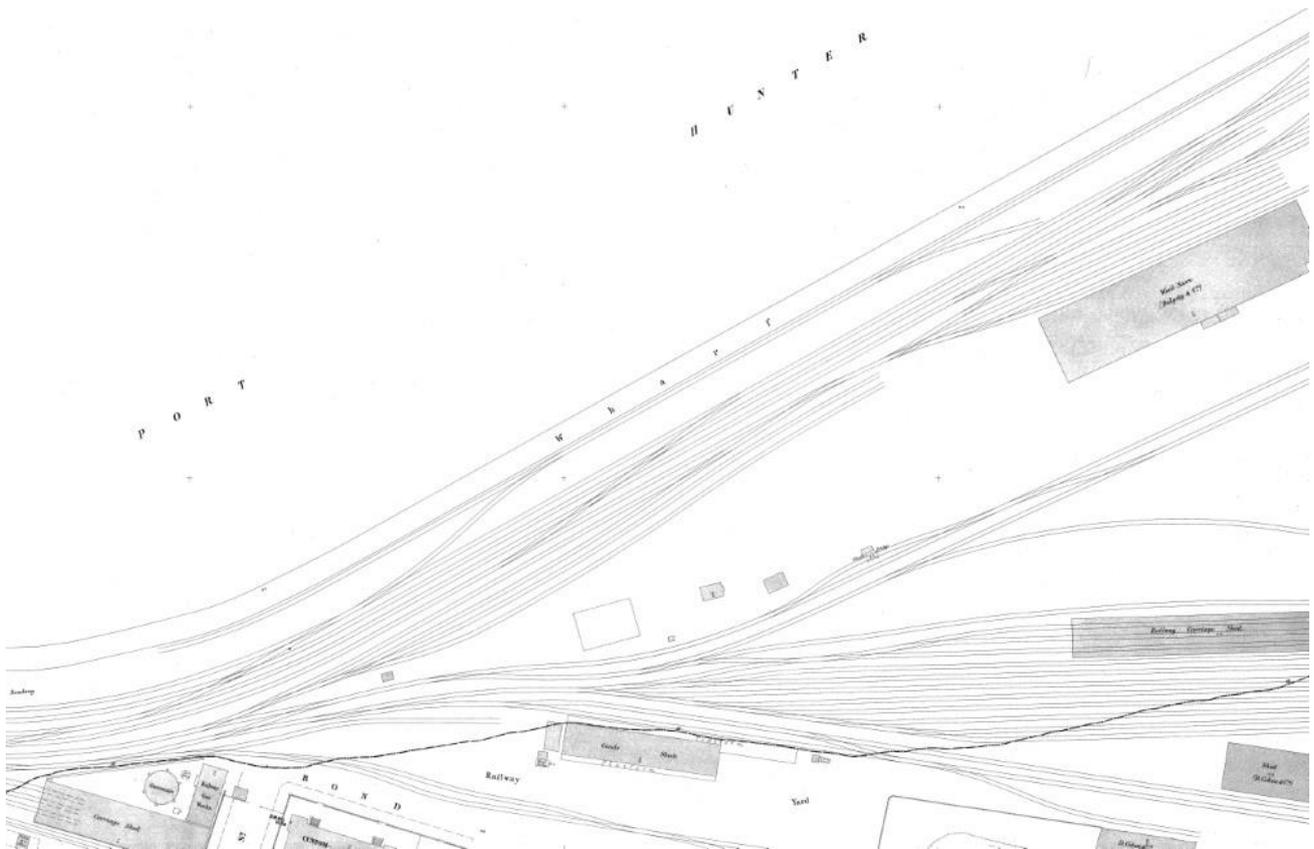
Detail shows rail lines, Goods Shed, stables and other structures within the Foreshore Park area (circled)

Source: State Records NSW Plan 1314 / Higginbotham 2013



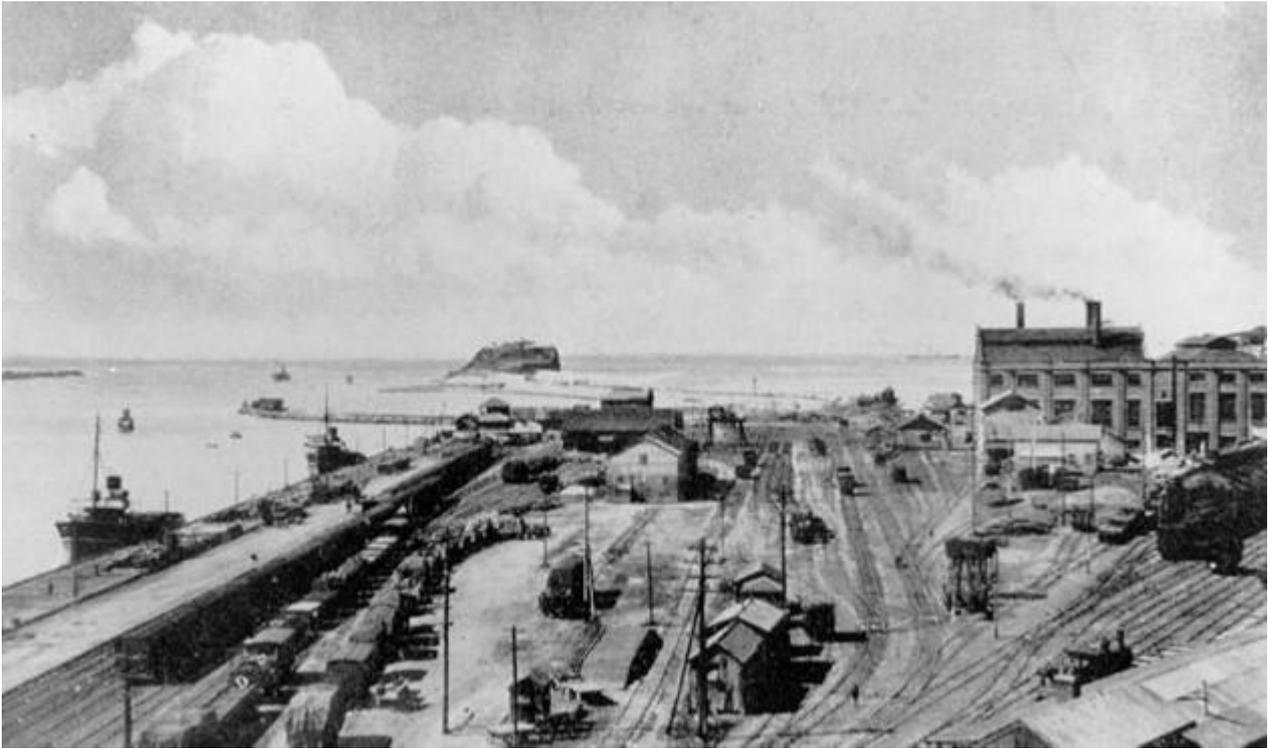
**Figure 4.10** Detail of approximately 1875 lithographic view of Newcastle  
Detail shows the development of the foreshore area in railyards following reclamation

Source: State Library of NSW



**Figure 4.11** Detail of 1895-1897 Newcastle Detail Series  
Detail shows the railyards and sheds in current Foreshore park area

Source: Hunter Living Histories



**Figure 4.12** Detail of 1924 Photograph

Detail shows the rail yards on Wharf Road and in Foreshore Park area

*Source: Hunter Photobank 09600056*



**Figure 4.13** Detail of 1966 Aerial Photograph

Detail shows the extensive railyards and the Zaara Street Power Station in the current Foreshore Park area

*Source: Hunter Living Histories*

## 5.0 Site Evaluation

The project area is located entirely within the western portion of Foreshore Park. As discussed, Foreshore Park was extensively re-landscaped in the late 1980s as one of Newcastle’s high profile projects for Australia’s Bicentennial Celebration.

Table 5.1 provides select photographs of the project area.

**Table 5.1 Project area photographs**

Photographs	
<p>View to southwest along northern boundary of project area. Project area on left of image. Wharf Road carpark (constructed 2017) on right.</p>	<p>View to west showing current play ground equipment</p>
<p>View to northeast showing Landscaping within Foreshore Park / the project area</p>	<p>View to southwest along southern boundary of project area. Project area on right.</p>

**Photographs**



View to northeast across project area



View to south showing existing amenities building



View to northeast along northern boundary of project area



View to southeast across project area towards existing playground equipment (behind trees in background). Note existing utilities within project area.

## 6.0 Aboriginal Archaeology

As discussed in **Section 2.0**, a search of the Aboriginal Heritage Information Management System (AHIMS) identified no Aboriginal sites within or in the immediate vicinity the project area.

### 6.1 Due Diligence Assessment

The *National Parks and Wildlife Act Regulation 2019* (the Regulation) identifies that compliance with the *Due Diligence Code of Practice for the Protection of Aboriginal Objects in New South Wales* is taken to constitute due diligence in determining whether a proposed activity will harm an Aboriginal object. Section 87(2,4) of the *National Parks and Wildlife Act 1974* establishes that it is a defence to prosecution under Section 86(2) (the strict liability offence) if due diligence was exercised to reasonably determine that the activity or omission would not result in harm to an Aboriginal object. The following section discusses Aboriginal cultural heritage in consideration of the due diligence code.

#### 6.1.1 Assessment of Aboriginal Archaeological Potential

Section 8 of the *Due Diligence Code of Practice for the Protection of Aboriginal Objects in New South Wales* outlines the process to guide due diligence assessments, summarised below in relation to the proposed works.

##### **Will the activity disturb the ground surface or any culturally modified trees?**

Yes. The proposed works will involve ground disturbance within the project area. However, the project area comprises reclaimed land, with land reclamation primarily occurring from the mid nineteenth century. Prior to land reclamation, the project area was located within the harbour; to the north of the original shoreline which ran along what is now the southern side of Scott Street, crossing the Newcastle Railway Station site and following the approximate line of Bond Street and Stephenson Place.

The project area does not contain any original vegetation. There is no potential for culturally modified (carved or scarred) trees to be impacted by the current proposal.

##### **Are there any:**

- a) **Relevant confirmed site records or other associated landscape feature information on AHIMS?**
- b) **Any other sources of information of which a person is already aware?**

As discussed, a search of AHIMS identified no site records or other associated landscape feature information on AHIMS within the project area and immediately surrounding area. Sites are located to the south on what was the foreshore area (refer to **Figure 2.1**).

**c) Landscape features that are likely to indicate the presence of Aboriginal objects?**

The due diligence code identifies landscape features that indicate the likely existence of Aboriginal objects as including areas located on a ridge top, ridge line or headland, within a sand dune system and/or within 200 metres of waters on land that is not disturbed land.

The project area is located approximately 70 metres south of the current alignment of the harbour. Prior to modification (reclamation) of the area, the project area would have located entirely in the harbour / within the Hunter River (based on historical mapping). From the mid-1800s the area has been extensively modified through reclamation. As such the project area is located within an artificial landform created as a result of land reclamation.

In accordance with Clause 80B of the National Parks and Wildlife Regulation, land is defined as disturbed if it has been the subject of human activity that has changed the earth's surface, being changes that remain clear and observable. The area of proposed works meets the description of disturbed land. Under Step 2B of the due diligence code, the area therefore does not meet the definition of a landscape feature likely to indicate the presence of Aboriginal objects.

On the basis of the information presented above, it is considered that there is a low likelihood that the proposed works will result in harm to Aboriginal objects. In accordance with the due diligence code, it is assessed that no further Aboriginal heritage investigations are required, and works can proceed with caution.

In accordance with the controls of Section 5.04.01 (Due diligence and development assessment) of the Newcastle Development Control Plan (NDCP) 2012 it is noted that an Aboriginal Heritage Impact Permit (AHIP) is not required.

In the unlikely event that unexpected Aboriginal archaeological objects are encountered during the course of works, recommendations in **Section 8.0** will apply.

## 7.0 Historical Archaeological Potential

Historical archaeology in Australia generally relates to the study of the past using physical evidence in conjunction with historical sources. Historical archaeology is generally defined as comprising the period since European arrival in Australia in 1788.

An archaeological resource is the physical evidence of the past and may comprise sub-surface evidence including building foundations, occupation deposits, features, and artefacts. The historical archaeological potential of an area is the likelihood that there may be physical evidence relating to earlier phases of development and occupation beneath the current footprint of the site.

### 7.1 Newcastle Archaeological Management Plans

As discussed in Section 3.0, the NAMP 1997 and the AMP Review discuss the potential for evidence of pre-reclamation wharfage and boat harbours and the railway sidings located along the river frontage following reclamation.

### 7.2 Archaeological Potential of the Project Area

The archaeological potential of the project area comprises two main periods of historical use:

- Pre-reclamation
- Post reclamation

#### 7.2.1 Pre-reclamation

Much of the reclamation of the east Newcastle foreshore was carried out by the dumping of ballast from visiting ships, both domestic and international. Alternatively, ballast was reportedly used to form a wall behind which sand and silt were pumped from harbour dredges to form newly reclaimed land.

Deep sewer and stormwater works in Scott Street during construction of the Newcastle Light Rail exposed extensive deep reclamation fill west of Watt Street in the original foreshore area. Similar evidence of reclamation fill to that exposed during the Light Rail project was exposed within the former Bus Depot area to the rear of Newcastle Station during Hunter & Central Coast Development Corporation's Public Realm Landscaping Project (Umwelt 2021). The reclamation fill essentially comprises sandstone rubble in a silty sand to depths of over two metres (refer to **Table 7.1**).

While no evidence of early boat harbours, jetties or the early enclosing of the harbour foreshore has been exposed during works in the area, it is noted that the project area crosses the former location of the jetty connecting the 1833 constructed ballast wharf to the shoreline. The jetty was likely constructed by 1840 (refer to **Figure 7.1**). Dependant on Hunter Water's requirements for the existing sewer there is potential

for deep excavation associated with connecting (or relocating) the sewer to expose remains of the 1840s jetty.

**Table 7.1 Archaeological Evidence of Reclamation**

Archaeological Evidence of Reclamation	
 <p>View to east towards Watt and Scott Street intersection showing sandstone rubble reclamation fill beneath Scott Street. Newcastle Station is immediately to left of photograph.</p>	 <p>View to south, west of the intersection of Scott and Bolton Streets, showing sand and silt reclamation fills beneath Scott Street immediately adjacent to Newcastle Station. Scale = 2 metres</p>
 <p>View to south showing excavation alongside Watt Street. Top of rubble reclamation fill exposed beneath compact gravel hard stand base. Scale = 500 millimetres</p>	 <p>View to south showing excavation alongside Watt Street. Top of rubble reclamation fill exposed beneath compact gravel hard stand base. Scale = 500 millimetres</p>



**Figure 7.1** Detail of 1846 Plan of the City of Newcastle

Project area shown in red outline. Note location of jetty connecting the Ballast Wharf to the shoreline

Source: Newcastle Regional Library – Local Studies Section LHMB 333.3/16

## 7.2.2 Post Reclamation

By the 1870s extensive rail yards were established in the area of current Foreshore Park; with rail lines, a weigh bridge, goods shed (including associated platform) and other likely shed-like structures located within the project area (refer to **Figure 7.2**).

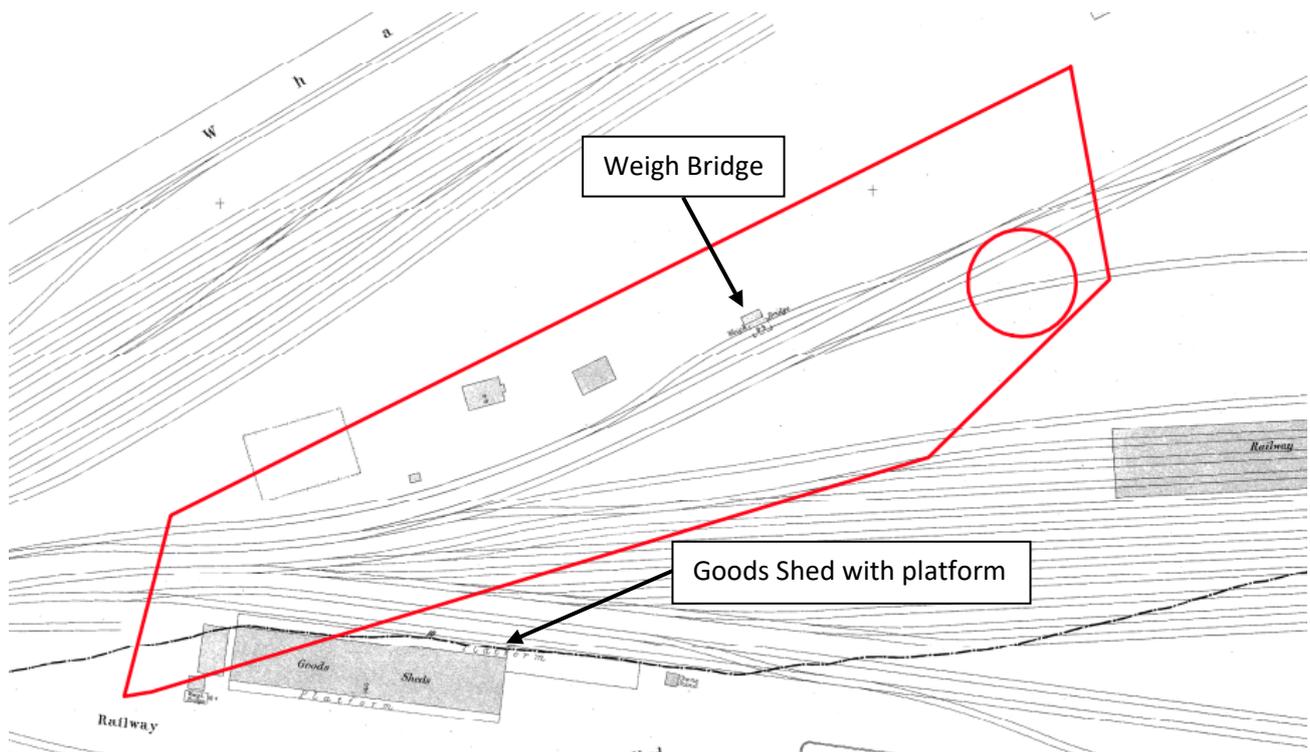
Archaeological monitoring was undertaken of the civil works required for the circuit of the inaugural Newcastle 500 Supercars event in 2017; including along Wharf Road and within Foreshore Park. An approximately 45 by 6 metre area of concrete footings was exposed along the south side of Wharf Road, on the northern edge of Foreshore Park. The footings were likely associated with a wool store located at the end of the former wharf area wool sidings. In addition, a valve associated with the former Zaara Street Power Station, a rail siding concrete pad and a brick stormwater pit were exposed (refer to **Table 7.2**).

Archaeological evidence of railway sheds have also been exposed recently:

- The redevelopment of Civic Station undertaken as part of the Museum Precinct exposed the brick footing of the 1872 constructed Carriage Shed (Umwelt 2020a).
- Archaeological evidence of the 1880s constructed carriage shed was exposed at 4 Merewether Street (Umwelt 2020b).
- Archaeological evidence of the footings of an 1880s Rail Carriage Shed was exposed within Newcastle Station during Hunter & Central Coast Development Corporation's Public Realm Landscaping Project (Umwelt 2021).

Such archaeological evidence confirms the accuracy of the historical plans, photographs and descriptions of the construction of rail yard sheds in newspapers such as the Maitland Mercury and Hunter River General Advertiser, in particular the detailed 1890 Waterboard plans (refer to **Figure 7.2**). In 1890 the Survey Section of the Land Department was commissioned by the Hunter District Water Board produce the series of large-scale plans of urban Newcastle, for planning purposes. The first plans were produced in 1894 and show detailed footprints of buildings present at the time. Some of the plans were later revised to reflect an approximately 1920 footprint. The plans cover a large portion of Newcastle, from the east end as far west as Lambton, providing accurate information of all buildings (and other features such as wells and cisterns) present at the time (Russell Rigby 2019).

As such, additional similar archaeological remains are considered likely to only be able to provide limited new information and are unlikely to be considered of high research potential or significant archaeological 'relics'.



**Figure 7.2** Detail of 1895-1897 Newcastle Detail Series

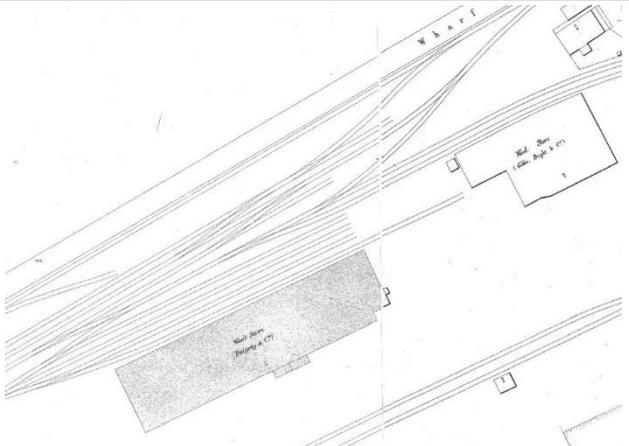
Project area outlined red

Source: Hunter Living Histories

**Table 7.2** Former Railyard Archaeological Remains

Former Railyard Archaeological Remains	
	
<p>View to west showing area of strip footings exposed in Foreshore Park Road in 2017</p>	<p>View to northwest showing concrete lined pit and adjacent machine bases exposed in Foreshore Park Road in 2017</p>

**Former Railyard Archaeological Remains**



Detail of 1890s Hunter District Water Board Plan showing former structures and rail lines located within Foreshore Park to the east of the project area. The concrete footings exposed during construction (2017) of the carparks in Foreshore Park along Wharf were likely the footings of the large shed at the bottom of the image.

No rail lines were exposed – these were likely entirely removed during the landscaping for the Bicentennial.



View to northeast showing large concrete pad exposed in Foreshore Park Road in 2017  
(Scale = 2 metres)



View to north showing concrete lined pit with a cast iron valve exposed in Foreshore Park Road in 2017. The valve is likely associated with the Zaara Street Power Station inlet and outlet water cooling channels.



Stormwater pit exposed in Foreshore Park Road in 2017.  
(Scales = 1 metre)

### 7.3 Heritage Act Relics Provision

Historical archaeological potential is defined as the likelihood that a site may contain physical evidence related to an earlier phase of occupation, activity, or development. Physical evidence of an archaeological site could be in the form of building foundations, occupation deposits, features and artefacts.

Archaeological resources are irreplaceable. They have the potential to contribute to our knowledge and understanding of early history using information that is unavailable from other sources.

However, some below ground archaeological remains are not considered to be relics; but are instead referred to as works or engineering works. Examples of items that can be considered works rather than relics include culverts (drains), historic road formations, historic pavements, buried roads and tramlines or rail lines (RMS 2015 and Higginbotham 2013). Although an approval under the relics provisions of the Heritage Act may not be required, any such archaeological remains still need consideration of appropriate management dependant on their significance. Although an item may not be considered a relic itself, relics may be present associated with it (for example in the form of artefacts).

### 7.4 Conclusion

The potential below ground archaeological remains within the project area are likely limited to evidence of:

- Post reclamation rail yard structures
- Reclamation fill
- Pre-reclamation evidence of the enclosing of the harbour

It is noted that any reclamation fill is likely to be of considerable depth. Preliminary geotechnical investigation for the project area identified fill depths of between 1.2 to 4.6 metres (Douglas Partners 2021). We understand additional geotechnical investigation is proposed. The results of the investigation should be considered in terms of potential depths and locations of deep reclamation fill.

There is not considered to be potential for significant artefacts (associated with public refuse disposed into the harbour during its use or remediation of the harbour). The majority of the proposed works are unlikely to be of sufficient depth to expose refuse deposited during its use.

In relation to the potential to expose archaeological remains the proposed works are generally considered to be minor in nature with limited depth of below ground disturbance except for the potential sewer works and any deep remediation that may be required. It is considered unlikely that significant historical archaeological 'relics' will be exposed during the proposed works.

However, it is acknowledged this does not negate the potential for archaeological remains to be exposed associated with a pre-reclamation 1840s jetty (refer to **Figure 7.1**), reclamation fill or the post reclamation railyards to be present.

With the exception of the 1840s jetty, such archaeological remains are not expected to be intact or of high research potential and are considered likely to only be able to provide limited new information at best.

It is appropriate for the proposed works to be undertaken in accordance with an exception to subsections 139(1) and (2) of the Heritage Act 1977; with this report as supporting documentation.

#### **7.4.1 Exception to S139(1) and (2) of the Heritage Act 1977**

An exception from the requirement for an excavation permit can be granted for disturbance or excavation of land, provided it is for minor works that have minimal impact on relics of local heritage significance.

Minor works with limited impact are considered to comprise works having a small or negligible impact on relics of local heritage significance.

This can be demonstrated by addressing that either:

- the scale or nature of the proposed excavation, regardless of the depth, would not adversely affect the relics and deposits or
- only affect less important areas of an archaeological site and not areas of highest potential (e.g., cesspits, occupation deposits, wells) or
- the scale of works is otherwise consistent with a limited impact, when the precise location of the archaeological relics is known, the area affected by the works is a small portion of the total known area of potential for relics

Heritage NSW guidelines state that if works are considered to be minor and have a limited impact on relics of local heritage significance, this decision must be documented, in addition to the works themselves, and the records kept for a reasonable time in accordance with the general conditions as gazetted 18 February 2022 (Government Gazette of the State of New South Wales Number 59–Planning and Heritage).

## 8.0 Conclusions and Recommendations

It is determined that:

- The proposed activities can proceed with caution without requiring further Aboriginal heritage investigations
- The proposed works should be undertaken in accordance with an exception to subsections 139(1) and (2) of the Heritage Act 1977; with this report as supporting documentation.
- Dependant on Hunter Water's requirements for the existing sewer there is potential for deep excavation associated with connecting (or relocating) the sewer to expose remains of the 1840s jetty. It is noted that any other deep excavation or remediation work also has the potential to expose pre-reclamation remains. The results of the proposed additional geotechnical investigation may provide further information relating to the potential for the 1840s jetty.

As such, the proposed works can proceed without any further historical archaeological or Aboriginal archaeological management being undertaken with the exception of the following recommendations:

- The proposed works should be undertaken in accordance with an exception to subsections 139(1) and (2) of the Heritage Act 1977; with this report as supporting documentation.
- Deep excavation associated with the connection (or relocation) of the sewer should avoid, if possible, the former location of the 1840s jetty structure (refer to **Figure 7.1**). If deep excavation in the former location of the jetty can not be avoided an archaeologist should be consulted with to determine the appropriate archaeological management.
- In the event that any potential archaeological 'relics' are discovered during works all work in the area shall cease immediately and a qualified archaeologist will be consulted to determine an appropriate course of action prior to the recommencement of work in the area.
  - in accordance with an exception to subsections 139(1) and (2) of the *Heritage Act 1977* this will likely comprise archaeological documentation.
- All parties involved in the works must aware that it is an offence under Section 86 of the NPW Act to harm or desecrate an Aboriginal object unless that harm or desecration is the subject of an Aboriginal Heritage Impact Permit.
- In the event that any Aboriginal objects are identified within the area of works, works within the immediate vicinity of the Aboriginal object should cease and a suitably qualified archaeologist should be contacted to confirm the validity of the object. Should the object be confirmed to be of Aboriginal cultural origin, Heritage NSW must be notified so that appropriate management strategies can be identified.

- In the unlikely event that a potential burial site or potential human skeletal material is exposed, work in the vicinity of the remains is to halt immediately to allow assessment and management. If the remains are suspected to be human, it will be necessary to contact local police and Heritage NSW to determine an appropriate course of action.

## 9.0 References

Douglas Partners 2021. Preliminary Geotechnical Investigation and Waste Classification, Harbour Foreshore Masterplan Wharf Road, Newcastle

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Umwelt 2020b. Archaeological Report Newcastle Light Rail Project

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